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PLEASANT HILL DOWNTOWN



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PLEASANT HILL DOWNTOWN

DRAFT: March 1990

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Downtown Specific Plan

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CHAPTER I

INTRODUCTION

The Pleasant Hill Downtown Specific Plan provides a long-term strategy for realizing the development potential in Downtown Pleasant Hill. This strategy is comprised of several tools the city has had and still has to use to facilitate the development of a well organized, clearly identifiable and economically viable downtown. The need for an orchestrated planning strategy has been most strongly felt when one recognizes the difference between the disjointed development that resulted without a plan and the character of development that occurred under previous plans. This plan builds upon and refines the city's previous efforts to develop a Downtown Plan.

DEVELOPMENT HISTORY

Pleasant Hill began as a ranching and agricultural settlement on land granted from the Mexican government in the 1800's. Population growth was very slow as the original grant parcels were divided and ranching and farming remained the dominant land use.

The earliest recorded residential subdivisions appeared between 1920 and 1930 on the south side of Gregory Lane, west of Contra Costa Boulevard. This area is included in this Specific Plan.

Beginning immediately after World War II, major residential development began. Subdivisions were built in close proximity to the established traveling routes, such as Contra Costa Boulevard, then a State Highway. Streets were relatively narrow and drainage was above ground.

In 1961 Pleasant Hill incorporated as a city. The first General Plan, adopted in 1962, envisioned a population of 60,000 in 20 years. In 1966, the city amended its General Plan to include a Core Area program establishing the downtown business district. The purpose of the amendment was to present a revitalization plan for improving and developing the city's Business Core Area.

In 1973, the City Council established the Pleasant Hill Redevelopment Agency for the purposes of implementing the Core Area Plan development. The area was characterized by a variety of industrial, retail commercial, office, multi-family and single family residential uses. The objective of the Pleasant Hill Commons Redevelopment Plan, as it became named, was to create a distinct downtown district with an environment of trees, water and recreational elements.

The initial Pleasant Hill Commons Redevelopment Plan area encompassed three distinct "super blocks" which were to develop individually. The area was planned to be developed incrementally with each block to be tied together through control of design and land use patterns to create an attractive, compatible downtown area. Under the guidance of the 1973 Plan, several residential projects,

the Pleasant Hill Plaza Shopping Center, the Two Worlds Complex, Gregory Centre offices, and the Terraces Office building were developed. In general, development was achieved in a phased program around the periphery of the redevelopment area leaving the more ambitious and difficult to assemble Central Blocks for a final phase.

In 1983 the Commons Plan was revised to respond of changing economic, environmental and social conditions in Central Contra Costa County. The amendment allowed for greater building height and intensity.

Subsequently in 1986, an initiative was approved by the voters to redirect development policy's contained in the 1983 Plan. The initiative established height limitations, density of development standards and office building bulk limitations. The General Plan was subsequently revised in 1990 to reflect the requirements of Measure "B". The standards established in the initiative have been included in this plan.

SITE LOCATION AND DESCRIPTION

The Downtown Specific Plan consists of 52 acres of land within the City of Pleasant Hill, California (see Figure 1). The area is generally bounded Interstate Freeway 680 on the east, Boyd Road on the south, Cleaveland Road on the west and Gregory Lane on the north. This area still contains a patchwork of strip commercial, light industrial, retail, office and residential uses, interspersed with vacant or under-utilized property.

The existing surrounding development, the new City Hall, the existing roadway network, visibility from the freeway and vistas from the site provide both constraints and opportunities for Downtown Pleasant Hill.

The site is relatively flat, lying in the beautiful Diablo Valley. Mount Diablo and the Briones Hills provide a dramatic, natural backdrop which invites incorporation of landscaping and other environmental elements into the plan. The new City Hall provides an architectural anchor to the site from which the Downtown can radiate. Figure 2 illustrates a summary of constraints and opportunities.



SITE LOCATION

FIGURE 1

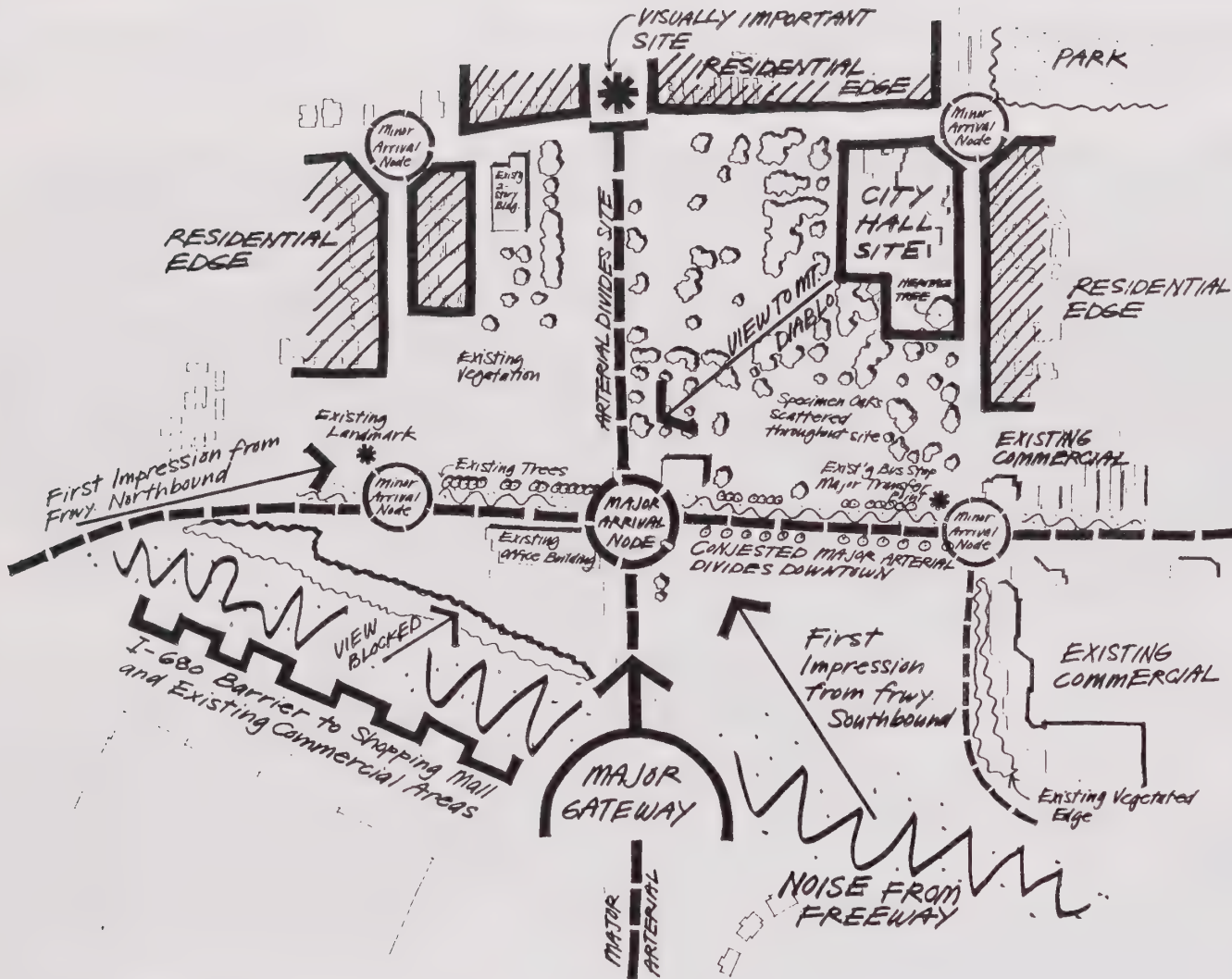
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P L E A S A N T H I L L D O W N T O W N
C I T Y O F P L E A S A N T H I L L , C A L I F O R N I A



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OPPORTUNITIES AND CONSTRAINTS

FIGURE 2

P L E A S A N T H I L L D O W N T O W N

CITY OF PLEASANT HILL, CALIFORNIA

0 300 600 FEET

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PURPOSE AND INTENT OF THE SPECIFIC PLAN

Under California Law (Government Code Section 65459 et al.,) cities and counties may use Specific Plans to develop policies, programs and regulations to implement the jurisdiction's adopted General Plan. Specific Plans often function to coordinate individual development proposals within a defined area.

The law requires that a Specific Plan include text and diagrams specifying:

The distribution, location and intensity of land uses, including open space, within the plan area.

The distribution, location and capacity of infrastructure, including transportation, sewage, water, storm drainage, solid waste and energy systems.

Standards and criteria for development and utilization of natural resources.

An implementation program, including capital improvement plans, regulations and financing strategies.




Specific Plans are intended to be vehicles for implementing the goals and policies of a community's General Plan and can only be adopted or amended if they are consistent with the jurisdiction's General Plan and is shown in Figure 3.

This Plan has been adopted as a separate chapter in the General Plan, by resolution, and is sufficiently detailed to be adopted by ordinance to provide underlying zoning for the Downtown Plan.




KEY

SINGLE FAMILY RESIDENTIAL


DU's per Net Acre

	LOW DENSITY	1.3 to 3.0
	MEDIUM DENSITY	3.1 to 4.5
	HIGH DENSITY	4.6 to 6.9

MULTIFAMILY RESIDENTIAL

	LOW DENSITY	7.0 to 19.9
	MEDIUM DENSITY	20.0 to 29.9
	HIGH DENSITY	30.0 to 40.0





COMMERCIAL

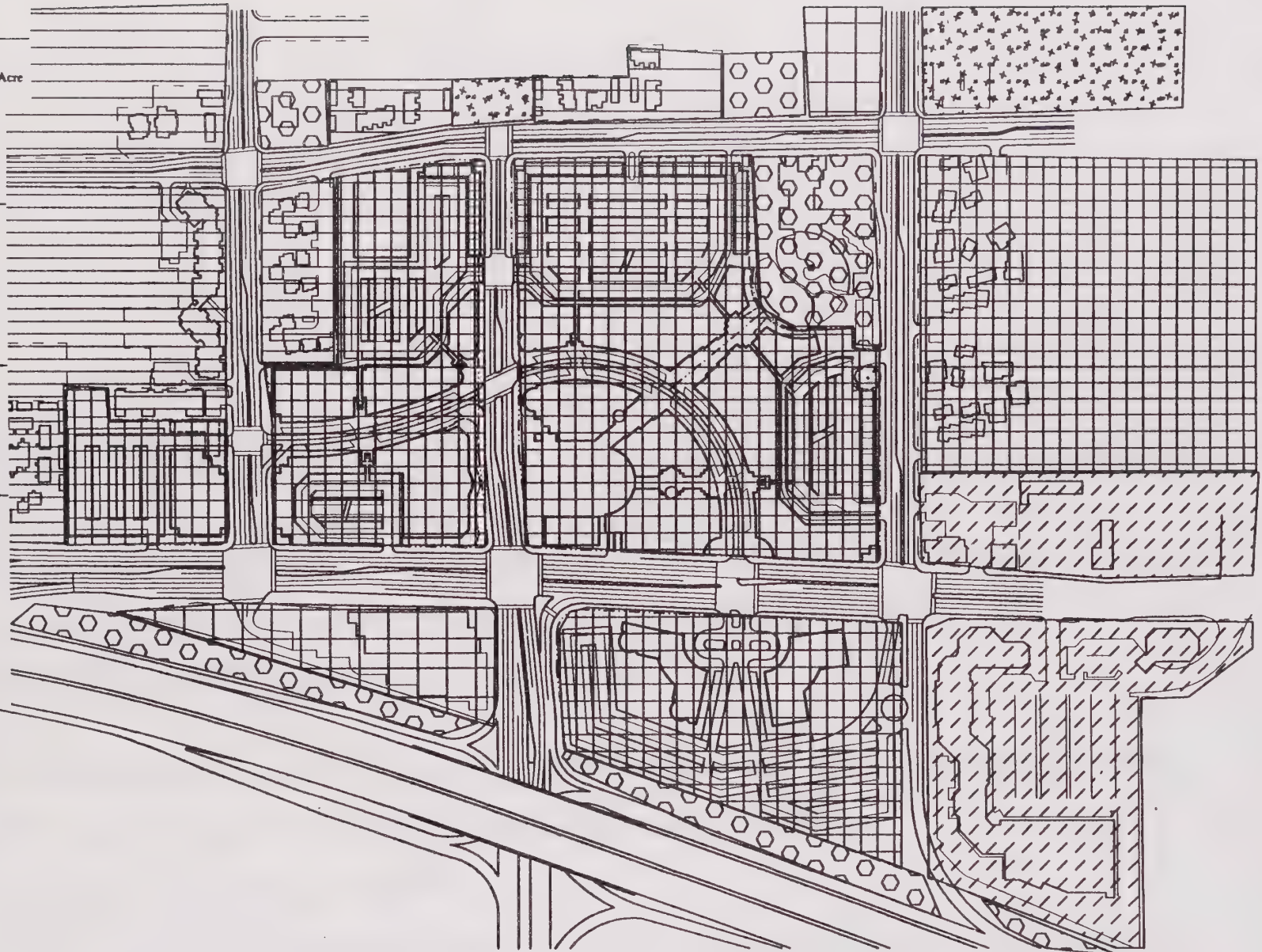
	COMMERCIAL/RETAIL
	MIXED USE
	OFFICE

INDUSTRIAL

	LIGHT INDUSTRIAL
--	------------------

PUBLIC AND SEMI-PUBLIC

	PARK
	OPEN SPACE
	SEMI-PUBLIC & INSTITUTIONAL
	SCHOOL



GENERAL PLAN

FIGURE 3

PLEASANT HILL DOWNTOWN
CITY OF PLEASANT HILL, CALIFORNIA

0 300 600 FEET

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CHAPTER II

DOWNTOWN SPECIFIC PLAN

During the development of the latest General Plan in 1989, a citizen survey was conducted by the city to determine what type of development was desirable for the downtown. Many meetings subsequently occurred and were devoted to the process of building a consensus. This was finally reached and is represented in the General Plan's Goals and Objectives. The goals included in the General Plan will guide the development program of the Downtown Specific Plan.

The Specific Plan is intended to provide the reader with a clear and concise statement of city policy. Much of the supporting documentation for this plan is found in the Pleasant Hill General Plan and its Environmental Impact Report and the Environmental Impact Report for this plan.

The Goal provides the philosophy for the Specific Plan. From this single statement, the direction for the policies shall be set.

The city policies for the downtown are found in the Guiding Policies which are the city's statements which refine the philosophy of the goal. Taken together, the Guiding Policies describe the vision that the Plan seeks to achieve. These policies establish the basis for the implementation measures specified in the Plan.

Finally, Implementation Measures represent a commitment to actions consistent with the Guiding Policies. Implementation measures reference existing programs and establish new ones which translate the guiding policies into a functional framework for physical development decisions.

GOAL

The goal of this Specific Plan is:

Develop a Downtown as a competitive retail and commercial center with sufficient public amenities to establish an identifiable sense of place for the City of Pleasant Hill.

GUIDING POLICIES

The General Plan included five downtown policies which are stated below.

Encourage the development of a special retail environment in the Downtown for Pleasant Hill.

Promote a compatible mix of retail, civic, office and residential uses.

Strongly encourage ground floor retail uses.

Develop a Downtown which complements the City Hall complex.

Encourage the Redevelopment Agency to actively implement the goals of the Downtown Plan by revising the Redevelopment Plan for the Downtown, if necessary.

IMPLEMENTATION MEASURES

Based on the information generated from community input, staff, consultants and potential developers regarding population and economic growth, traffic and circulation constraints, responsible land use planning, and the development of a downtown image, the guiding policies have been refined to include the following implementation measures.

Land Use

- Overall development should be limited to 1,200,000 square feet or less of usable floor area (excluding residential development and structured parking).
- Locate a majority of new retail development between Gregory Lane, Cleaveland Road, Boyd Road and Contra Costa Boulevard.
- Encourage hotels, retail, offices, residential and a mixture of these uses in the downtown.
- Establish minimum criteria for the size of each new development proposal.
- Encourage retail, specialty retail, night time and entertainment uses in the Downtown Plan.

- . Provide for a City Hall within the Downtown.
- . Identify a parcel(s) for hotel development.
- . Provide a comprehensive open space and paseo system for the Downtown.
- . Limit office development to a maximum Floor Area Ratio of .40.
- . Allow a Floor Area Ratio of .75 in areas with a mix of office, retail or residential use.
- . Create positive land-use relationships that:
 - Maximize visual accessibility from main public spaces;
 - Maximize flexibility of tenant space;
 - Provide parking within a reasonable proximity to uses.

Circulation

- . Develop a main street which serves both vehicle and pedestrian traffic.
- . Provide adequate internal vehicle circulation which does not encourage through traffic which would adversely affect the pedestrian oriented retail environment.
- . Promote public transit amenities such as benches, passenger shelters and bus turn outs in the downtown area.
- . Limit new curb cuts and access drives on Contra Costa Boulevard and consolidate existing ones over time.

Parking

- . Develop joint parking facilities for all development sites, where possible.
- . Reduced parking ratios shall be permitted for joint utilization of downtown parking facilities when parking is provided in shared lots.
- . Structured parking is preferable to on-grade, surface parking.
- . Discourage or prohibit on-street parking except on Crescent Street.
- . All parcels with non-structured or non-subterranean parking facilities shall be landscaped with a minimum of 15% of the site area devoted to landscaping.

Urban Design

- . Develop a comfortable scale and character for a small town downtown.
- . Discourage large areas of blank walls without fenestration or other scale articulation facing pedestrian walkways.
- . Encourage the use of pedestrian paths or paseos.
- . Minimize building setbacks to enhance an urban and pedestrian environment.
- . Outdoor dining and "festival activities" shall be encouraged within specially designed plazas.
- . Entries to retail and service uses shall be encouraged along pedestrian walkways.
- . Encourage provision of public pedestrian access through buildings.
- . Five story building shall be given priority on the east side of Contra Costa Boulevard.
- . Maintain and enhance perimeter arterial landscape medians.

- . Special design attention shall be given to the building facades facing parking structures.
- . Buildings should take advantage of scenic views and sunlight areas.
- . Two or three story building facades shall be encouraged along the Crescent Street to increase the intensity of activity, and add visual interest to the street.
- . Building elements which add scale and interest such as bay windows, parapets, decks, terraces, arcades, canopies, exterior stairwells and cornices shall be encouraged.
- . Diversity of design and themes to enhance visual interest shall be encouraged.
- . Except for major stores, which will serve as anchors to the Downtown, larger buildings should be divided into multiple store fronts in order to reflect a small scale retail character.
- . Creative and attractive display windows should be encouraged at ground floor to enliven the pedestrian street environment.
- . Festive banners, awnings, and lighting shall be included in the design of the downtown and permitted within or over the public right-of-way.
- . First floor building facades on the Crescent Street shall be 40% transparent, (i.e., glass, open or display windows).
- . Signs shall be creative, of good quality and designed to be coordinated.
- . Special design guidelines for signing shall be developed and enforced for the Downtown.
- . Enhance the visual identity at corners and intersections with architectural interest and active uses.

Open Space and Public Art

- . Provide meaningful usable, public areas.
- . Connect public open spaces through tree lined passages linking the City Hall to the rest of the Downtown in the site plan.
- . Provide for pedestrian needs by ensuring wide sidewalks, public seating and street furniture.
- . Take advantage of solar orientations to create comfortable outdoor spaces.
- . Provide for land uses that support the use of adjacent outdoor public spaces.
- . Create an urbane setting appropriate for public gatherings.
- . Ensure that sculptures, fountains and other art forms are an integral part of the Downtown design.
- . Develop an identifiable and usable Downtown Plaza.

CHAPTER III

THE PLAN

As has been noted earlier, the City of Pleasant Hill, like most suburban communities that developed immediately after World War II, did not have an overall master plan to develop a central core area, nor did one develop naturally. The city has recognized the need to develop an area which contains the critical elements that makes a city an identifiable place. The modern, suburban city needs to have a mixture of commercial elements that complement the values of the citizenry. The passage of the 1986 Measure "B" initiative and 1990 revision of the General Plan has led a movement towards what is considered the traditional small town downtown as the desirable solution for the creation of an identity. In an age in which the shopping mall or suburban office park has been the prevalent type of commercial development, Pleasant Hill is now looking toward a pedestrian scale development more appropriate to the perceived character of the community. This plan is drawn to provide a link between suburban values and the traditional downtown concept.

THE CONCEPT

The Downtown concept is a traditional Main Street retail community center. The plan is based on short blocks with ample pedestrian amenities and parking in close proximity. Conventional single use zoning is replaced with a mixed use plan more indicative of juxtaposed land uses found in early 20th Century downtowns. (See Figure 4).

The following sections of the Specific Plan discuss each of eight designated planning areas. From the discussion, the impression might be left that each area is a separate entity in which development could be unrelated to the others. This is the antithesis of the plan. Rather, each of the areas are closely linked by the pedestrian paseo system, the traffic circulation system, parking and urban design. Planning Areas B, C, D, E, F and H are in fact one development that has been divided into parts to show their key characteristic to the whole. Table 1 allocates development to each of the planning areas, but when the actual development plans are presented, it is likely that density will be reallocated to improve the overall functioning of the Downtown. Therefore, both for Table 1 and later in Table 2, it needs to be recognized that the square footage distributions are illustrative and not absolute. The key is that the total development respects the overall limits and parking as appropriately distributed.

Table 1

Developable Areas and Floor Area Ratios

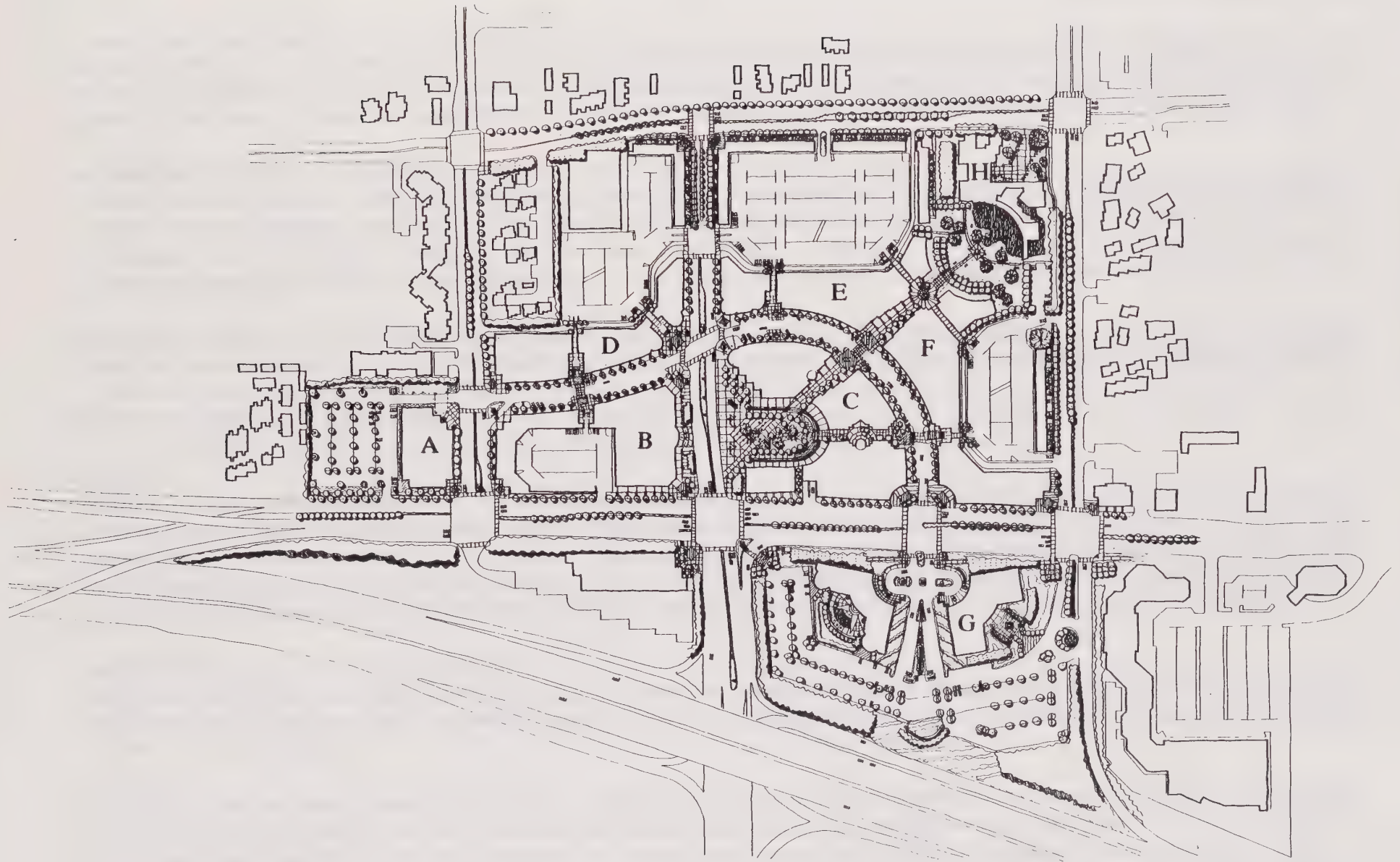
Planning Area	Net Acres	Allowed ² Square Footage of Development	Floor ² Area Ratio
A.	3.03	52,800	0.40
B.	3.57	155,700	1.00
C.	5.08	221,500	1.00
D.	5.96	155,600	0.60
E.	6.95	181,500	0.60
F.	5.38	140,600	0.60
G.	9.28	242,000	0.60
H.	2.82	36,800	0.30
	-----	-----	-----
	42.08	1,186,500 ¹	0.65

¹ Includes 135,000 square feet of residential and 120,000 square feet of hotel uses.

² Allowed square footage can be reallocated among area B-F and G.

Planning Area A - North Main Street Office

The Specific Plan proposes to utilize the residual land from the North Main Street realignment for a signature office building or destination commercial or recreation use. The parcel site is approximately 3.03 acres in size, and can be developed with a Floor Area Ratio of .40, resulting in a low to mid-rise office building with surface parking. Slightly higher density could be considered for a retail or recreation use which enhances the Downtown Plan retail scheme. Planning for this area should be sensitive to the key gateway location from the South. Additionally, the building will be the terminal focus of the Crescent Street. A focal element or other design feature shall be integrated into the site plan. Special designs consideration shall be given to screen adjacent residential uses from the impacts associated with office, retail or recreation uses.



CONCEPT PLAN

FIGURE 4

0 300 900 FEET

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P L E A S A N T H I L L D O W N T O W N
C I T Y O F P L E A S A N T H I L L, C A L I F O R N I A



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Planning Area B - South Downtown Plaza Retail

This area represents an opportunity to provide an expansion of the Downtown Plaza area to and across Monument Plaza. This block's role is to unify and enhance the main commercial street and the Downtown Plaza. Because of its relationship with the public square, this 3.57 acre area should have the same density as the central block, which shall be the highest allowed by the Plan.

To achieve this density, consideration for parking and buffering of uses need to be taken into consideration. The eastern parking structure shall be designed to have a facade which is attractive when viewed from Contra Costa Boulevard and the Terraces office building.

It is critical that the uses fronting on Monument Plaza be in concert with the Downtown Plaza environment, which is recreational and social in nature. This street frontage is particularly well suited for theaters, entertainment retail, restaurants and other evening and weekend uses.

A multiple screen cinema complex with approximately 3,000 seats, plus ancillary entertainment retail, will provide a great opportunity as a generator of people to populate and energize the Plaza. Formally aligned on the plaza, similar to the great opera houses of Europe, the building could be dramatically lighted with neon marquees and electronic scoreboard signing, giving the immediate area an aura of Times Square or Picadilly Circus - visible to the freeway and Plaza, yet shielded from the neighboring housing. The goal is to achieve the European concept of an evening's entertainment: dinner, a show, followed by lingering in a cafe and people watching.

Planning Area C - Downtown Plaza

One of the three major components of the Downtown Specific Plan is the establishment of a public plaza which becomes the principal focus of the Core Area. The other two components are the proposed Crescent Street and City Hall.

This block needs to have a pedestrian scale with closely knit multiple story buildings. Many small American cities frequently provide a downtown block, surrounded by moving traffic, as a park for the public. This plan takes this concept a step further by providing a large town square sculpted with water and vegetation, immediately adjacent to activity where food, water, civic art, play space, and passive space are available for the Pleasant Hill user. The block cannot stand alone, it must be linked with the paseo system to both the main shopping street and City Hall.

Pedestrian malls are an American and European urban element. In America these occasionally fail due to their lack of "anchor" land uses and uncoordinated parking. In the case of Pleasant Hill, the main street, Downtown Plaza, and pedestrian paseo system will provide access to five major destinations: City Hall, the Plaza, the cinema, retail areas and parking entrances. In addition, the paseos

should be designed as retail walkways of the appropriate width for double-loaded retail, and should include internal landscaping and water elements.

The Downtown Plaza will provide a focal point to the plan and should be surrounded by restaurants, cafes, and food-related retail uses. Parking, on this block only, will be provided in one level below grade directly beneath the retail buildings that front onto the main street (Crescent Street).

Planning Areas D, E and F - Crescent Street

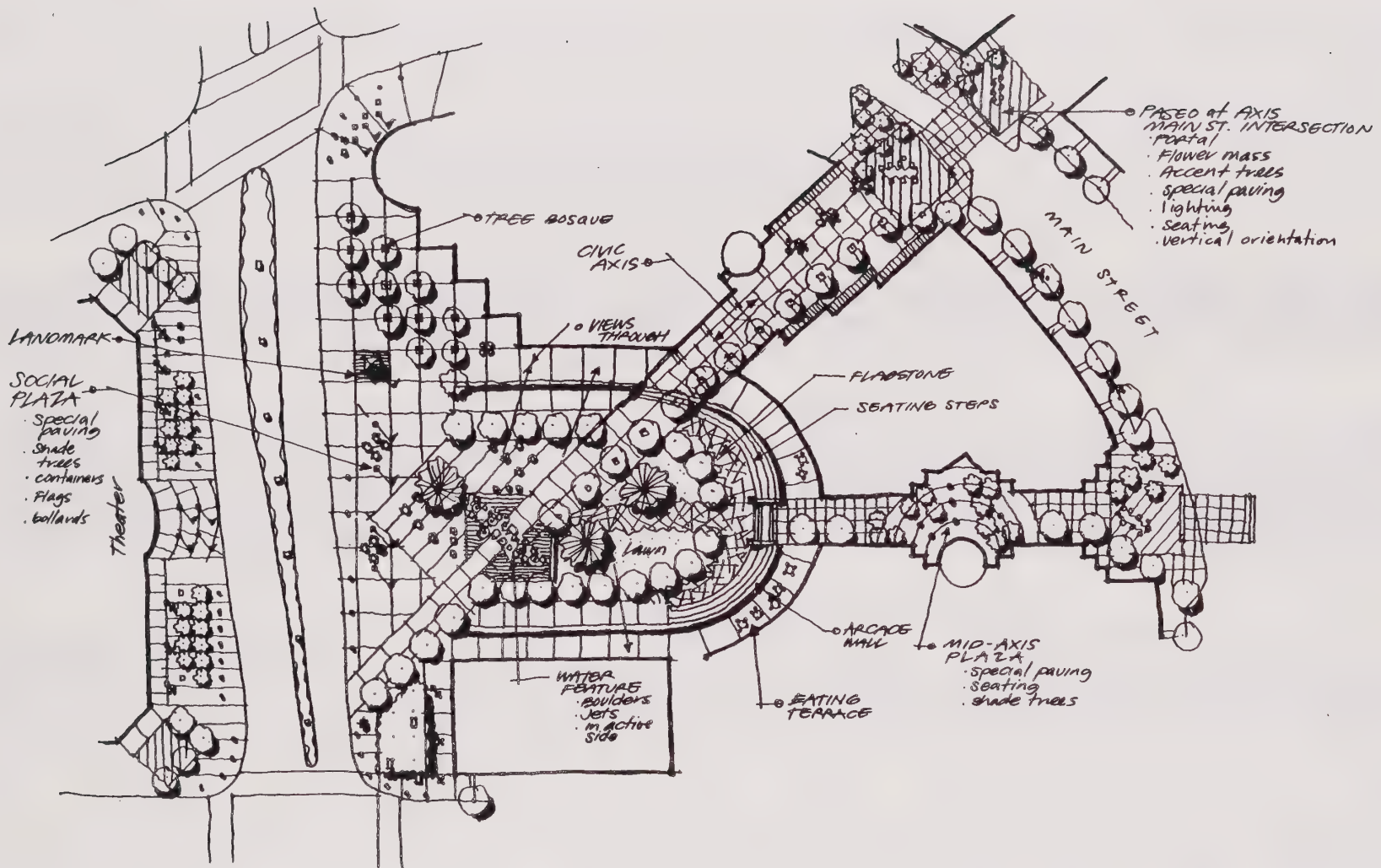
Crescent Street (the new main street) will be the principal shopping area in the Downtown. Stores should be located at appropriate positions on the site for a number of diverse types of retailing: anchor, mini-anchor, and single floor stores. In addition to the street entrances, access should be provided from the parking garages at the second level of the retail, then by stairs and elevator (for handicapped access) to the first floor retail. Specialty housewares and gourmet retail, a food store and/or drug store are envisioned as part of the retail component on the southern end of Crescent Street in Planning Areas D and B.

Adequate and accessible parking is an essential element for mixed-use development within a suburban context. To avoid the inherent problems of massive parking facilities within low-rise streetscape, parking structures will be sited at the edge of the project and surrounded by retail and residential buildings, so that the major visual evidence of parking will be the vehicular and pedestrian entrances to the parking structure. The Downtown Plan will also provide street parking throughout the development which will enhance the street energy and retail visibility so necessary to successful retailing.

Residential townhouses are proposed to mask the perimeter of the parking structures across the street from existing housing. For compatibility, these townhouses will be designed with landscaped edges, pitched roofs, chimneys, balconies, and other common indicators of residential construction. Access to each unit will be provided from street level, and from secure public lobbies opening on secured garage parking. Third level flats could be reached by elevator from the public lobbies.

A dedicated truck service street between the retail buildings and garage structures will provide service access to the majority of retail space, so that street loading mars neither the image of the Downtown nor safety of its streets. The truck access road will allow the garage structures to be designed with both landscaped and day-lighted areas.

To maintain the pedestrian scale, provide parking for other portions of the Downtown Plan, and to provide a transition from the higher intensity development along the freeway to the residential uses west, the plan envisions a lower floor area ratio in areas D, E and F than on the central blocks (B and C).



DOWNTOWN PLAZA

FIGURE 5

PLEASANT HILL DOWNTOWN
CITY OF PLEASANT HILL, CALIFORNIA

0 300 600 FEET

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STREET RETAIL

FIGURE 6



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PLEASANT HILL DOWNTOWN
CITY OF PLEASANT HILL, CALIFORNIA



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Planning Area G - East Side of Contra Costa Boulevard

When the Pleasant Hill Commons Plan was being amended in 1983, development on this site was shown to be constrained by inadequate circulation. At that time, it was proposed that the Gregory Lane off-ramp would be removed and relocated at Boyd Road and a major extension of Gregory Lane would provide access into the site. It was believed that this site could not be developed with uses that generated significant peak hour traffic generations.

The land use conclusions of the 1983 study are again supported in this plan, although the circulation system now proposed is significantly different. Rather than having off-ramps at Boyd Road, this plan envisions an urban diamond interchange at Monument Boulevard and retention of the Gregory Lane off-ramp. Due to weave patterns leaving the freeway, ingress and egress from Monument Boulevard to Area G will be prohibited. The retention of the Gregory Lane off-ramp will prohibit southbound left turns from Contra Costa Boulevard onto Crescent Street (old Trelany Road intersection).

As a result of traffic constraints, use of the site should be limited to uses of medium intensity office and hotel use.

A hotel facility with 250 rooms and a five-story office building could add the "critical mass" of pedestrians vital to the success of the Downtown Area during the daylight hours. These larger building masses are located so as to respond to the freeway, shielding the rest of the Downtown Plan from the associated noise and unpleasantness. Development on this site should provide a visual terminus to the eastern end of Crescent Street, while also allowing southbound views from I-680 to penetrate into the center of downtown. Two office buildings, totalling 240,000 square feet, could also be developed on this site.

Planning Area H - City Hall

In 1988, the City of Pleasant Hill purchased 2.8 acres to build a City Hall and small park area. The City Hall will be a major architectural statement and thus an identifying symbol to the city, while allowing the functions and activities that contribute significantly to the quality of life in the community.

The plan anticipates that the passive activity area and lake will be the terminus of the axial paseo system. The paseo will link the Downtown Plaza, Crescent Street and City Hall and unify the entire Downtown. The City Hall will be approximately 36,800 square feet and be the least intense area in the plan.

Table 2

Total Projected Development

Sub Area	Retail	Office	Residential Units (Sq. Ft.) ¹	Hotel (Sq. Ft.) ²	Civic Center	Total Square Footage ⁵
A.	0	52,800 ³	0	0	0	52,800
B.	155,700	0	0	0	0	155,700
C.	138,500	83,000	0	0	0	221,500
D.	86,600	39,000	30,000	0	0	155,600
E.	61,000	37,500	80,000	0	0	181,500
F.	77,000	38,000	25,000	0	0	140,600
G.	0	122,000	0	120,000 ⁴	0	242,000
H.	0	0	0	0	36,800	36,800
	518,800	372,300	135,000	120,000	36,800	1,186,500

1 Residential units are calculated at 1,500 square feet per unit.

2 Hotel rooms are calculated at 500 square feet per room.

3 Can also be commercial, retail or recreational with Use Permit.

4 Can also be office use.

5 Square footage and uses can be reallocated within areas B-F and G.



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CITY HALL

FIGURE 7



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P L E A S A N T H I L L D O W N T O W N
C I T Y O F P L E A S A N T H I L L , C A L I F O R N I A



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CIRCULATION

The major access to the Downtown is by Interstate-680 on the east. The major entrances to the Downtown are shown in Figure 8 and are the southbound off-ramp at Gregory Lane and Contra Costa Boulevard, the northbound off-ramp at Boyd Road and North Main Street and the proposed urban diamond interchange at Monument Boulevard. Secondary entrances are provided from the west along Gregory Lane and Boyd Road. The realignment of North Main Street with Contra Costa Boulevard will provide the southern entrance.

The intersection of Boyd Road and North Main Street will be realigned with Contra Costa Boulevard, thereby eliminating an awkward intersection between Cleaveland Road and Contra Costa Boulevard. The widening of North Main Street south of the Downtown improves the north-south circulation capacity, lessening the traffic demand on Cleaveland Road.

The circulation system for the Downtown discourages through traffic by narrowing Monument Boulevard west of the Crescent Street and minimizing the number of lanes on Cleaveland Road. Gregory Lane plan becomes the major east-west route from Cleaveland Road to Contra Costa Boulevard rather than as previously indicated in the Commons Redevelopment Plan. This reduction of traffic better utilizes the existing roadways and minimizes additional construction.

While most of the major circulation elements already exist, there are several programmed and funded improvements which will improve the overall circulation in the Downtown. The programmed improvements are shown in Table 3.

Table 3

Programmed Traffic Improvements

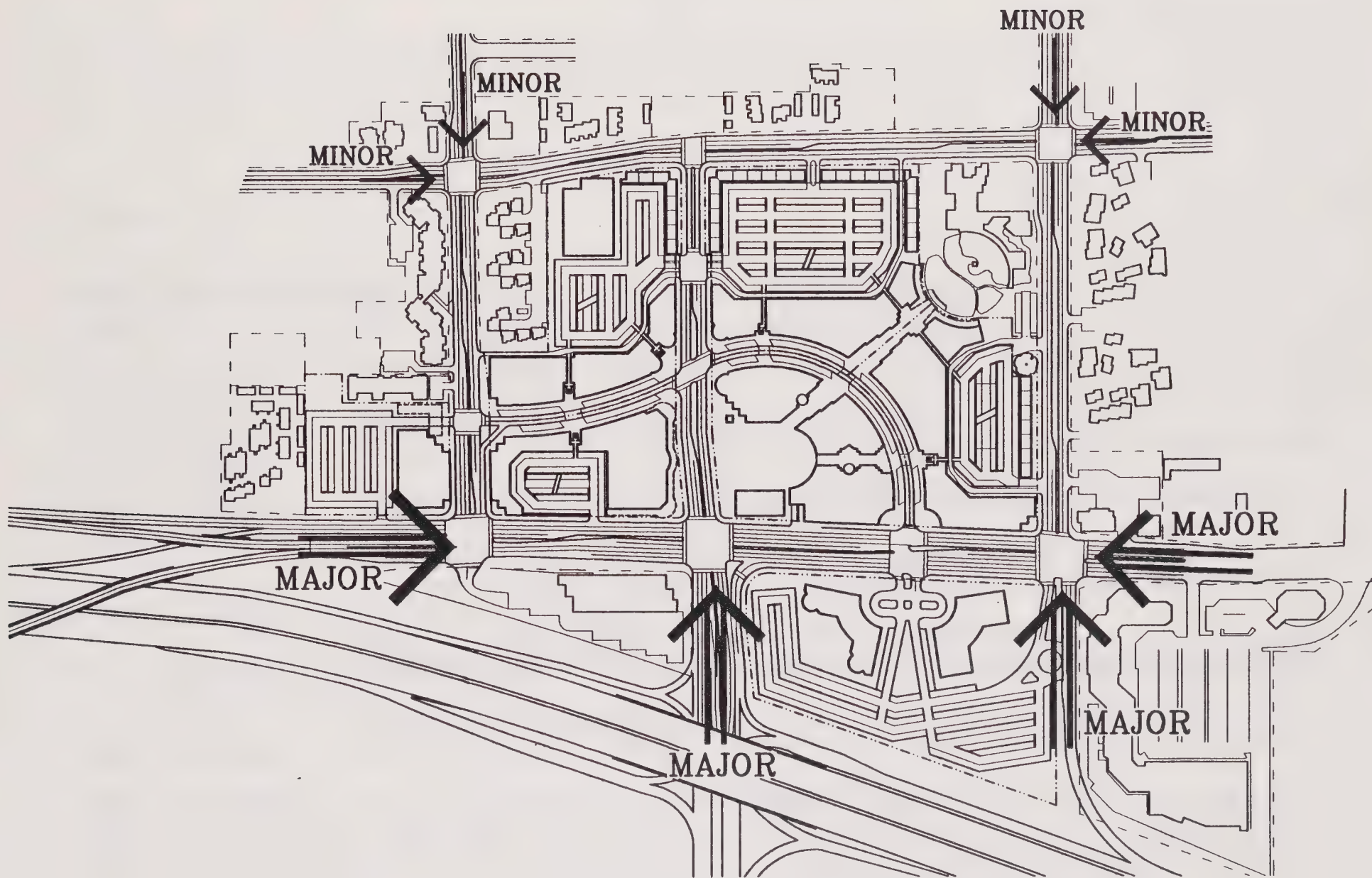
<u>Roadway</u>	<u>Improvements</u>
I-680	Widen to 10 lanes and reconfigure the Monument Interchange to an Urban Diamond. Modify the Contra Costa Boulevard off-ramp for direct access to North Main Street (Cal Trans projects).
North Main Street	Widen to 4 lanes and realign from Boyd Road to Oak Park Boulevard (Cal Trans Project).
Boyd Road	Widen to 4 lanes between Contra Costa Boulevard and Crescent Street (Cal Trans project).

Crescent Street in the plan replaces Trelany Road and is designed to accommodate the internal traffic demands of the Downtown. Crescent Street is intended to function as a main commercial street very similarly to Broadway in Walnut Creek or Santa Cruz Avenue in Menlo Park but will be a private street.

Projected Levels of Service

Traffic flows are heavily impacted by through traffic from Interstate-680 to Monument Boulevard in Concord and Bancroft Avenue in Walnut Creek. In 1989, the traffic congestion on Contra Costa Boulevard was caused by left turn movements from south-bound Interstate 680 to Contra Costa Boulevard and to east-bound Monument Boulevard. Short traffic signal spacing increased the congestion. Even without the major traffic improvements planned for the Downtown, the intersections were working at acceptable levels.

To evaluate and describe the way on intersection functions, the traffic engineers have developed a grading system, much the same as found in the schools. Table 4 describes the traffic report card standards. The existing and projected traffic level of services are found in Table 5.



ACCESS ROUTES

FIGURE 8



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C I T Y O F P L E A S A N T H I L L , C A L I F O R N I A



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Table 4

Levels of Service for Urban and Suburban Arterial Street

<u>Level of Service</u>	<u>V/C Ratios</u>	<u>Descriptions</u>
A	0.00-0.60	Free flow (relatively). If signalized, conditions are such that no approach phase is fully utilized by traffic and no vehicle waits through more than one red indication. Very slight or no delay.
B	0.61-0.70	Stable flow. If signalized, an occasional approach phase is fully utilized; vehicle platoons are formed. This level is suitable operation for rural design purposes. Slight delay.
C	0.71-0.80	Stable flow of operation. If signalized, drivers occasionally may have to wait through more than one red indication. This level is suitable operation for urban design purposes. Acceptable delay.
D	0.81-0.90	Approaching unstable flow of operations; queues develop but are quickly cleared. Tolerable delay.
E	0.91-1.00	Unstable flow or operation; the intersection has reached ultimate capacity; this condition is not uncommon in peak hours. Congestion and intolerable delay.
F	1.00+	Forced flow or operations. Intersection operates below capacity. Jammed.

Source: Highway Capacity Manual, Circular 212

Table 5

Levels of Service -- P.M. Peak Hour

Downtown Plan

<u>Location</u>	<u>Base 1989</u>	<u>Downtown Plan **</u>	<u>Downtown Plan With Regional Growth & Additional Mitigation</u>
I-680/Gregory Lane	D	D	E
Contra Costa/Monument	D	D	E
Contra Costa/Boyd	C	B	D
North Main/Boyd	--	*	*
Boyd/Cleaveland	B	A	A
Gregory/Cleaveland	A	A	B
Cleaveland/Monument	--	C	C

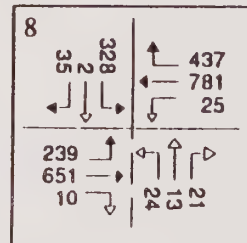
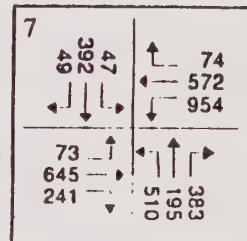
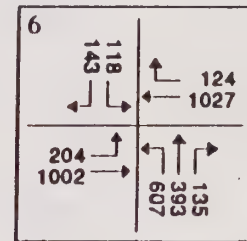
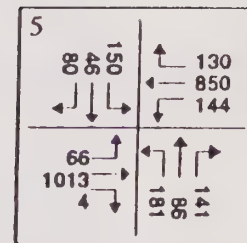
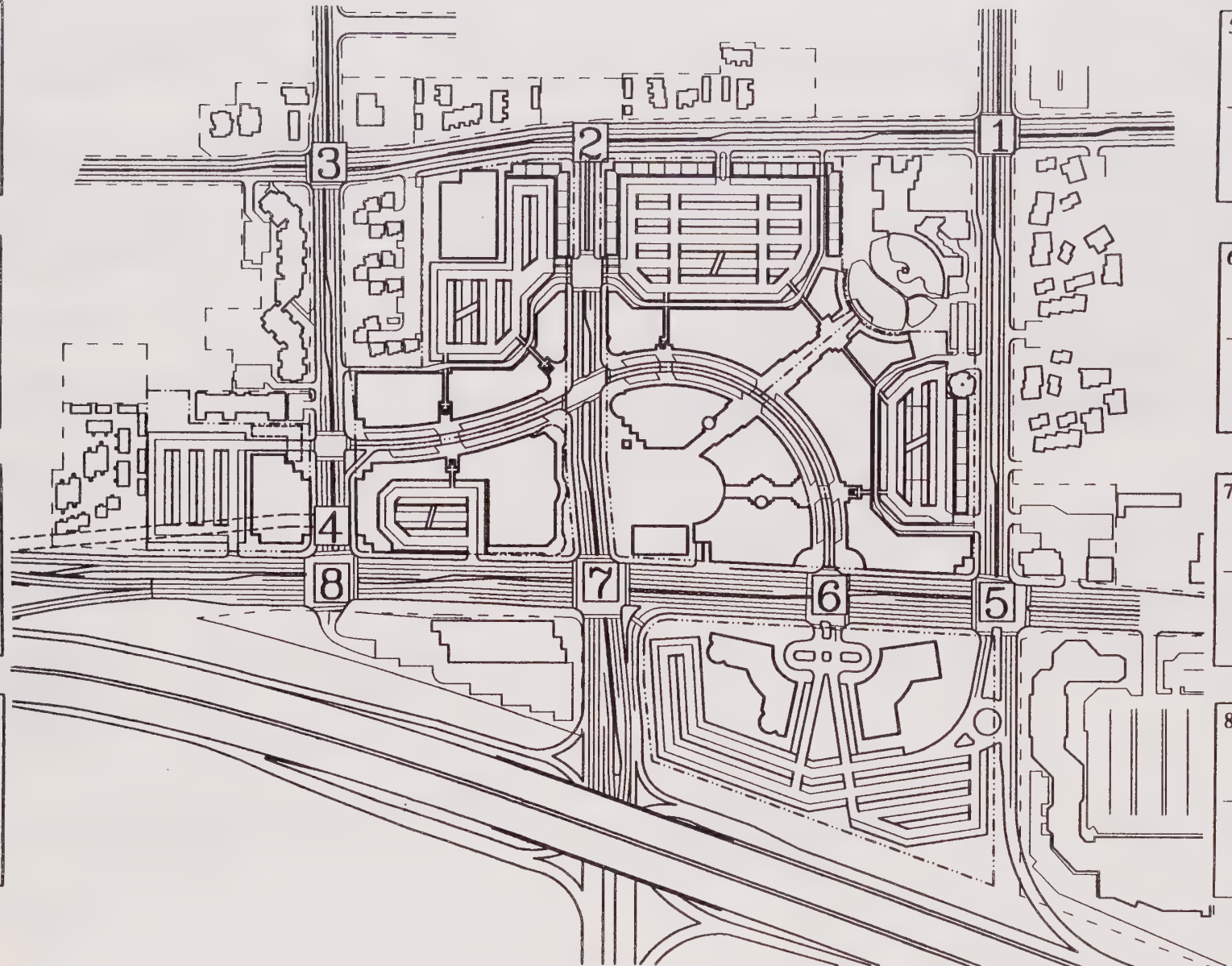
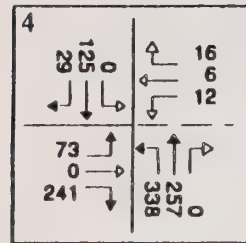
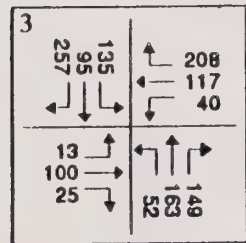
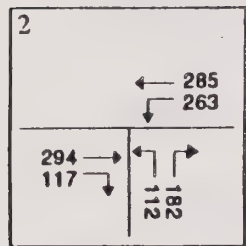
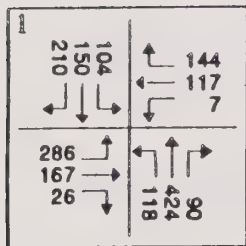
* This intersection is eliminated by the Downtown Plan .

** Based on existing conditions, project traffic and project mitigation (including Cal Trans Improvement Interstate-680). Does not include regional traffic increases.

Source: Omni-Means

The Downtown is designed to be the Central Business Districts for Pleasant Hill. In August 1988, the voters of Contra Costa County approved Measure "C" which designated appropriate levels of service for Land Use Classifications, including the Central Business Districts. Contra Costa Boulevard in the Downtown is designated as a Central Business District with an acceptable Level of Service of Low-E (90 to 94 V/C). Additionally, Contra Costa Boulevard is a route of regional significance because of its role as a major route connecting the cities of Central County. Figure 9 and 10 show the base (1989) traffic projects and those expected in the future.

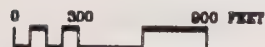
To ease the future traffic conditions, additional traffic improvements will be necessary. Table 6 summarizes the major improvements that are planned for the Downtown.



omni • means

BASE DAILY TRAFFIC VOLUMES

FIGURE 9

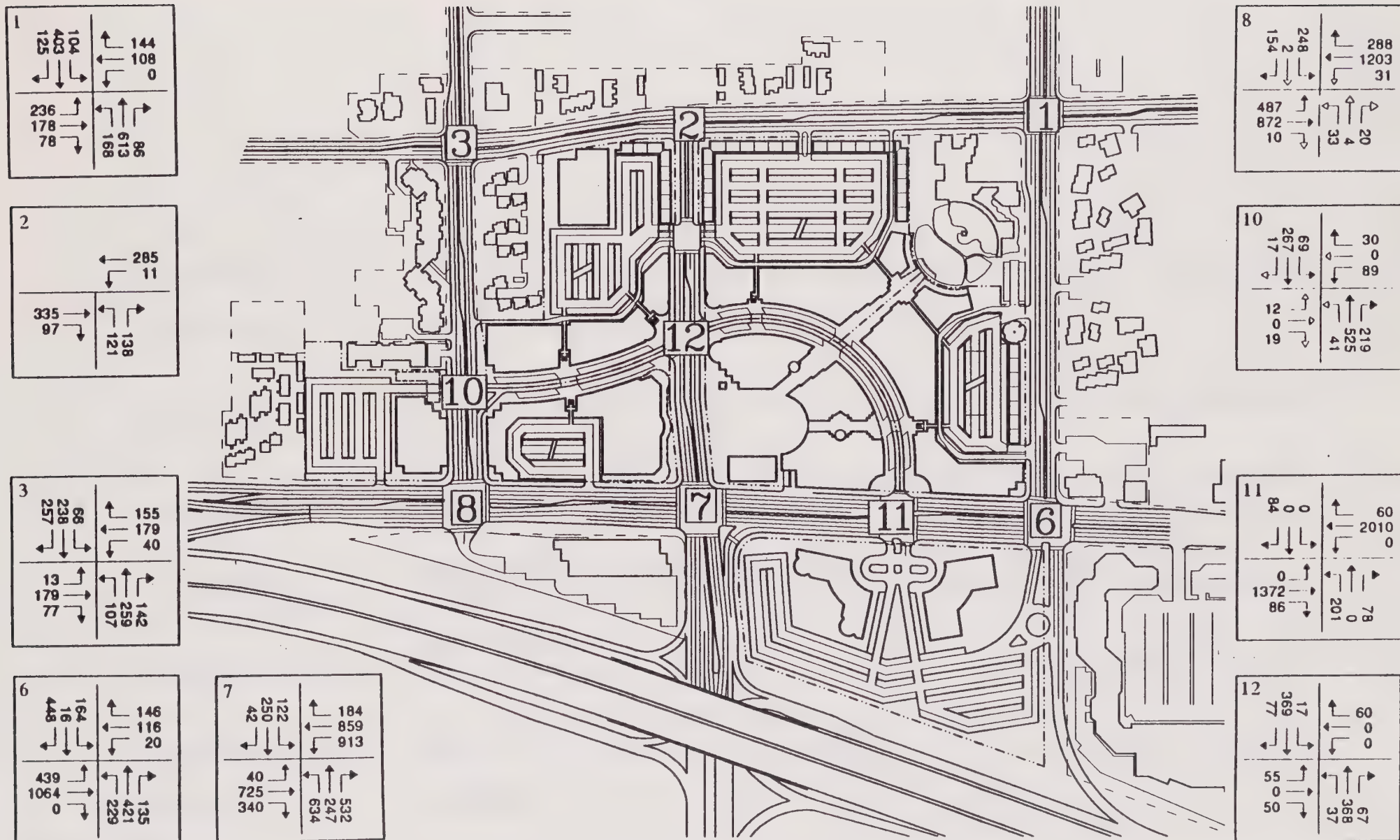


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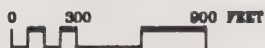
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PROJECTED TRAFFIC VOLUME

FIGURE 10



PLEASANT HILL DOWNTOWN
CITY OF PLEASANT HILL, CALIFORNIA

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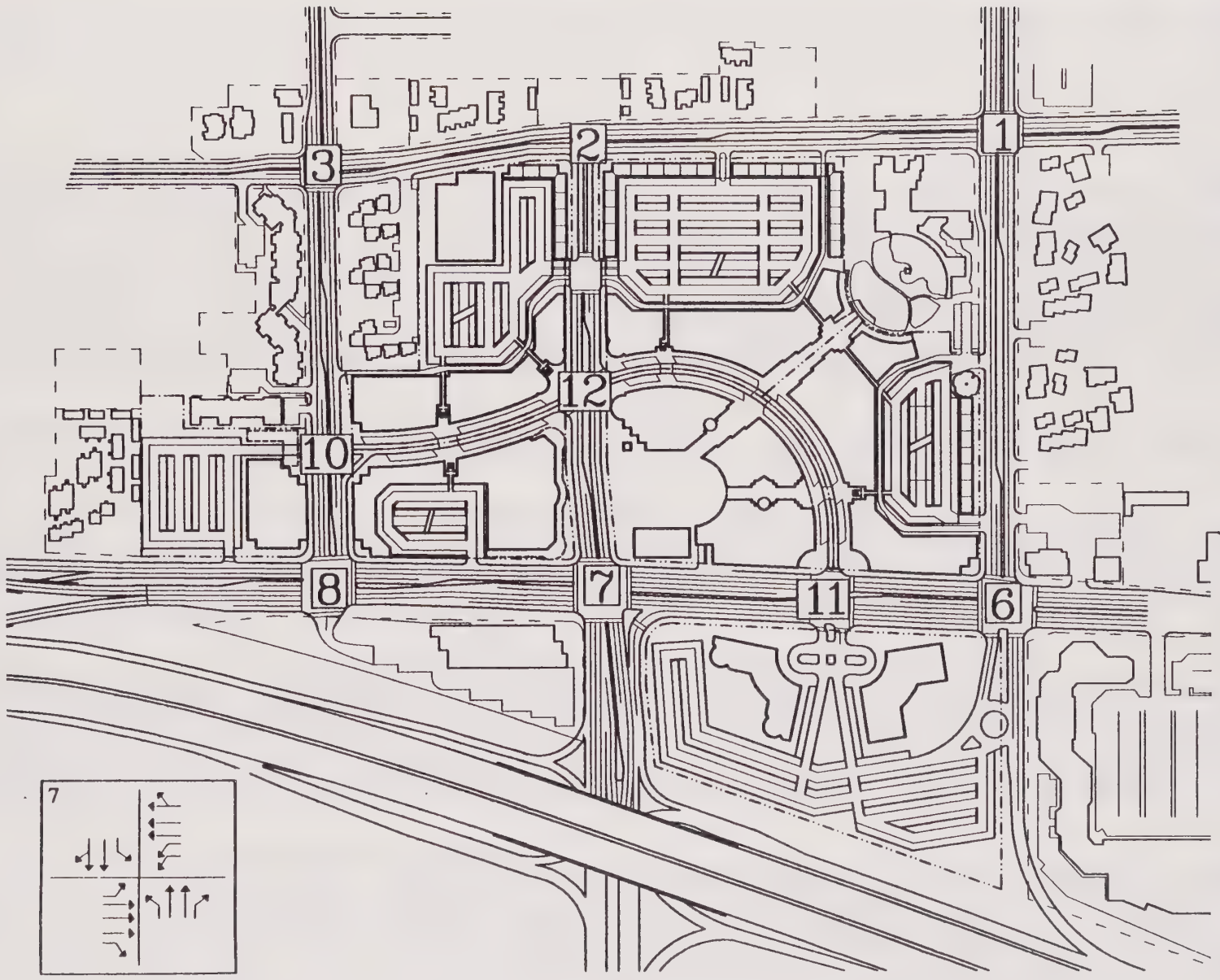
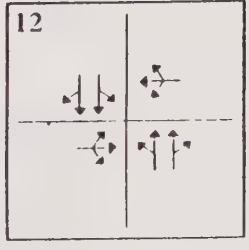
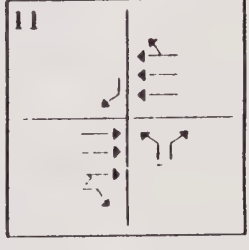
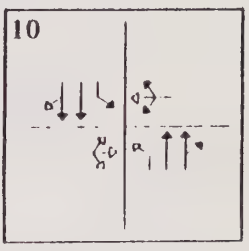
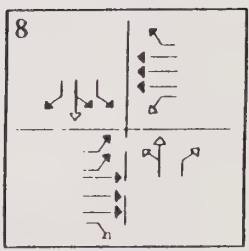
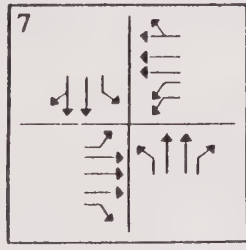
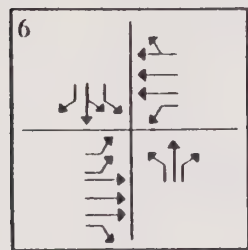
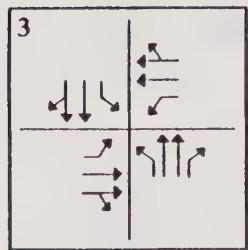
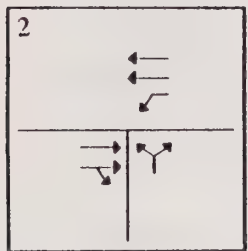
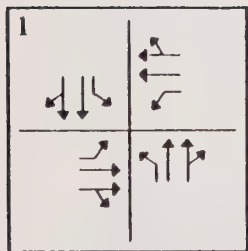


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Table 6

Future Projected and Regional Traffic Improvements

<u>Roadway</u>	<u>Improvements</u>
Monument Boulevard	Widen to 4 lanes eastbound between Contra Costa and I-680 and widen westbound to accommodate additional through lane (possible Cal Trans improvement).
Contra Costa Boulevard	Widen to include a new southbound lane from Gregory Lane to Boyd Road and left turn lane for access to subarea G at Gregory Lane. Northbound, add a new left turn lane for Boyd Road and right turn pocket to the Terraces. Between Monument Boulevard and Gregory Lane and right turn pockets for subarea G.
Gregory Lane	Eastbound, an additional lane for combined left turn and through movement to subarea G westbound. New left turn pocket for access to the parking garage in subarea F between Contra Costa Boulevard and Cleaveland Road.
Cleaveland Road	Widen to 4 lanes between Monument Plaza and Boyd Road. Add left turn lane at Monument Plaza.
Crescent Street	New road with two lanes and parking. To be a private street.
Monument Plaza	Widening to 4 lanes between Contra Costs Boulevard and access driveways to parking structures in subareas D and E.



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ROADWAY REQUIREMENTS

FIGURE 11



PLEASANT HILL DOWNTOWN
CITY OF PLEASANT HILL, CALIFORNIA



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Parking

The parking plan for the Downtown is designed to provide an adequate supply to accommodate peak parking demands. To assure that parking areas are used to their greatest extent, shared parking between commercial uses is necessary. Different land uses have parking demands that peak at different times. For example, offices have peak parking demands during the day while hotel parking peaks in the evening. To provide parking exclusively to meet the needs of each use separately would result in parking areas which are under utilized during portions of the day. At the same time, reliance on shared parking in cases where spaces are not convenient to each land use would result in localized parking shortages.

To maximize the parking efficiency, shared parking facilities are envisioned in the Downtown. To be successful, parking areas would need to be located within close proximity to the destination land uses. The size of the parking facilities will be determined by evaluating parking demand of the collective land uses. The overall parking demand calculations are based on the Urban Land Institute's (ULI) and the Institute of Transportation Engineers (ITE) work on shared parking. Demands are determined on the highest to peak conditions in a downtown, rather than the ratios established in the Zoning Ordinance. (See Table 5)

The analysis done for the Downtown Specific Plan examined the total parking demand during the P.M., both weekdays and weekends. The parking demand assumes that on weekdays, that there are not screening of the movies in the cinemas before 5:00 p.m. and that the hotel does not include banquet and conference rooms. The office and hotel site in Planning Area G, will be required to provide all necessary parking on that site, based on to the mixed use combination finally proposed.

Table 6 and Figure 12 show the parking capacity and distribution of the parking for each subarea. As can be seen from Table 6, there is an imbalance of parking demand and capacity for many of the planning areas.

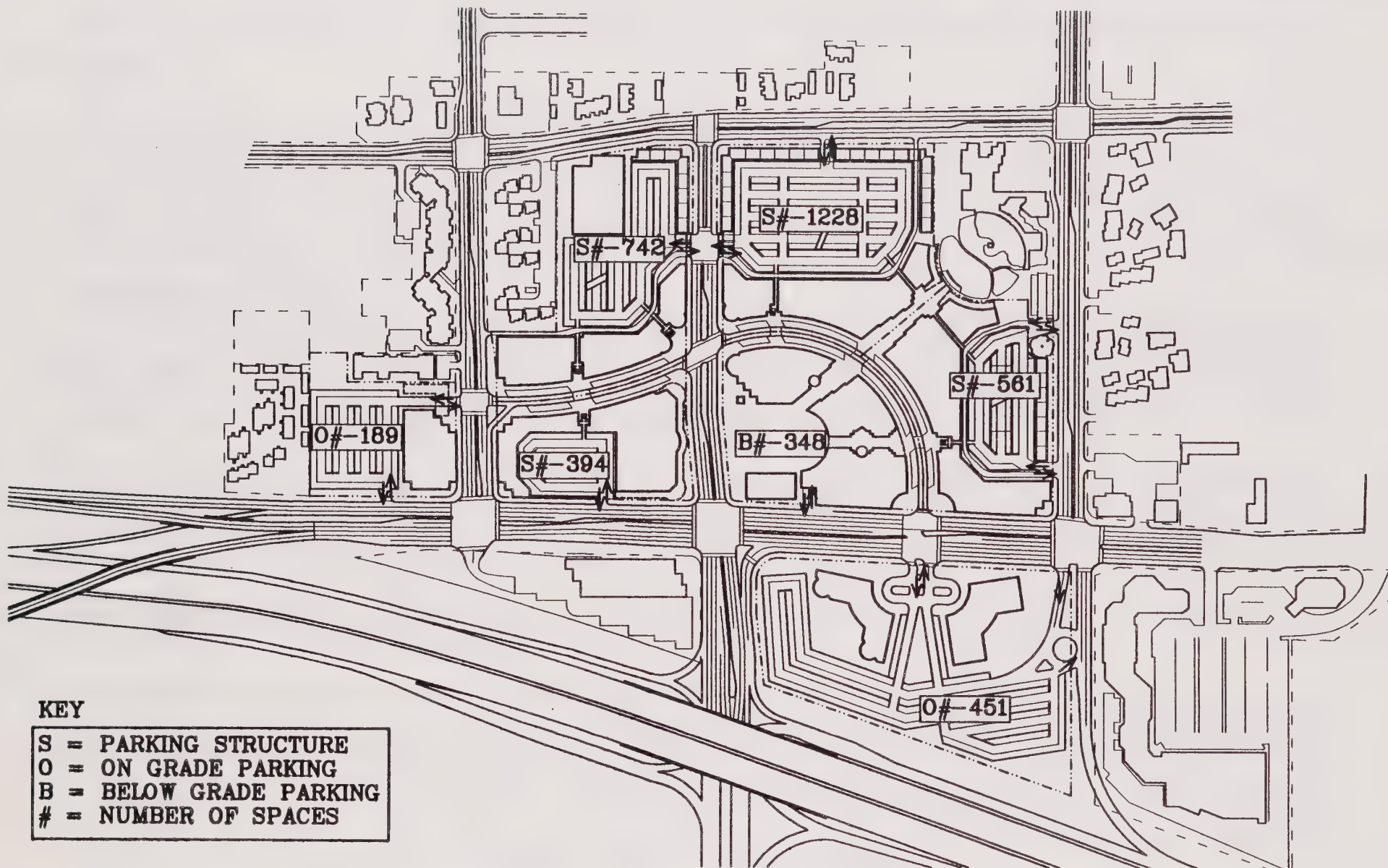
The accessibility of a parking area is a function of the purpose of each visit, duration of the stay, proximity to alternate parking areas, street access, perceived walking distance, safety and degree of weather protection. Many of the key factors have been analyzed in the site plan to minimize the negative influence and maximizes convenience for the user. Parking supply in planning areas "B" and "C" would numerically be substandard, however, these two areas parking demand are accommodated by the available parking in Planning Areas "D" and "E". Table 7 indicates where the customers are destined and where they would most likely park based on the plan. The total number of required parking spaces will be determined by analyzing the final plans, uses and square footages and will be calculated by using the shared parking formula.

Table 7

Zoning Ordinance Parking Requirements

Subarea	Retail 1:250	Restaurant 1:110	Office 1:250	Theater 1:30 Seats	City Hall 1:250	Hotel 1: Room	Residential 2:5 Unit	Other 1:25	Total
A	-	-	-	-	-	-	-	208	208
B	422	-	-	1000	-	-	-	-	1422
C	544	-	332	-	-	-	-	-	886
D	346	-	156	-	-	-	50	-	552
E	244	-	150	-	-	-	133	-	527
F	308	-	152	-	-	-	42	-	502
G	-	-	488	-	-	250	-	-	738
H	-	-	-	-	147	-	-	-	147
TOTAL	1874	0	1278	1000	147	250	225	208	4982

* Plus Restaurants and Banquets



KEY

- S = PARKING STRUCTURE
- O = ON GRADE PARKING
- B = BELOW GRADE PARKING
- # = NUMBER OF SPACES

PARKING LOCATION AND DISTRIBUTION

FIGURE 12



P L E A S A N T H I L L D O W N T O W N
CITY OF PLEASANT HILL, CALIFORNIA

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Table 8

**Mixed Use Requirements
For Peak Usage**

Subarea	Capacity	Peak Demand (2 PM Weekdays)
A	208	208
B	416	330
C	348	754
D	720	565
E	1215	695
F	572	629
G	457	426
TOTAL	3936	3606

Reference: 1 - ITE, "Parking Generation," 1987.

2 - ULI, "Parking Requirements," 1981.

3 - ULI, "Shared Parking," 1983.

TABLE 9

Parking Allocation

Customer Destination	A	B	C	D	E	F	G	TOTAL
A	70%	20%	--	10%	--	--	--	100%
B	--	75%	--	25%	--	--	--	100%
C (office)	--	--	100%	--	--	--	--	100%
C (retail)	--	--	80%	--	10%	10%	--	100%
D	--	--	--	100%	--	--	--	100%
E	--	--	--	--	100%	--	--	100%
F	--	--	--	--	--	100%	--	100%
G	--	--	--	--	--	--	100%	100%

Legend:

Customer Destination - actual work/shopping/home destination of motorists.

Parked Location - auto parked locations.

Pedestrian Circulation

When the pedestrian enters the Downtown, they enter a linear system of pedestrian paseos and midblock plazas; the scale of these narrow pathways is intended to contrast with the character of both the larger open spaces and the streetscape. These paseos will provide a diversity of possible uses which include specialty retail, galleries, and cultural exhibits, with commercial/office uses on the second story. These paseos direct pedestrian movement toward a range of possible destinations: City Hall, parking structures, retail and Crescent Street. The visual linkage between the paseo system and the streetscape will be achieved by creating a small mid-block plaza at the street where the paseo penetrates the building mass. Colorful, small-scale trees and other focal elements will work together with building architecture to emphasize this transition point.

The guidelines for the paseo system are as follows:

- . Widths of pedestrian circulation routes shall be as follows:
 1. Downtown Plaza/City Hall Paseo: 40-60 ft.
 2. Garage Access Paseos: 20-30 ft.
 3. Pedestrian/Auto-Oriented (Crescent Street) Street Sidewalks: 12-16 ft.
 4. Auto/Pedestrian-Oriented (Monument Plaza) Street Sidewalk: 8 ft.
 5. Auto-Oriented Street Sidewalk: 5 ft.
- . Create a unique image for pedestrian circulation between garage and plaza level. Relate this circulation to building lobbies or central plaza to create legibility and a sense of orientation.
- . Design consideration will be directed to the pedestrian crossing of Monument Plaza and Contra Costa Boulevard with safety in mind.
- . Design for barrier-free access for the elderly and handicapped.
- . Consider handicapped access in the selection of surface materials and path gradients. Gradients along major pathways shall not exceed 5% unless an alternative handicap access is provided.
- . Conform to State of California Title 24 Standards in design of all facilities.

Public Transit

The existing and planned transit services in Pleasant Hill have been identified in the General Plan. The Pleasant Hill BART Station is in close proximity to the Downtown. The Downtown and BART Station are linked by bus transit provided by the Contra Costa County Transit Authority (CCCTA). The same transit line that connects the Downtown with the BART Station, also connects it to Sun Valley Shopping Mall and Diablo Valley College. It is important that Downtown Pleasant Hill serve as one of the transit hubs for the Diablo Valley.

UTILITIES

Storm Drainage

The existing storm drainage system consists of a network of underground subsystems which drain west into Grayson Creek. The drainage system accepts water from east of I-680 and conveys the water through the Downtown. The amount of run off at peak drainage periods causes backups in the system and overflows from drainage and outlets.

Plans to divert drainage from east of the freeway and convey it to the Walnut Creek Channel have been explored. The diversion would eliminate much of the storm drainage problems. Even with this diversion several new drainage pipes will be needed along Boyd Road, Monument Plaza and Cleaveland Road.

The existing storm drains within easements and existing streets which are in conflict with the Specific Plan need to be abandoned, provided that abandonment, removal, plugging or relocation can be made. The city will make this determination at the time the development plans are submitted for review.

Waste Water

The existing waste water collection system consist of gravity flow lines serviced by Central Contra Costa County Sanitary District (CCCSD)

The existing sewer mains within Contra Costa Boulevard and Monument Plaza are capable of handling the expected demand of this plan and are in accordance with the CCCSD requirements.

The realignment and abandonment of Trelany Road and development of Crescent Street will have to meet CCCSD's requirements. Decision on the design of these facilities will have to be made at the time the development plans are submitted for review.

In addition to the existing storm drainage situation, much of the Downtown is within a 100 year flood plane, as designated by the Federal Emergency Management Agency (FEMA). This designation indicates that there is the probability of once in every one hundred years, the area will encounter flooding of about one (1) foot. To ensure development is above the potential flood levels, a minimum building pad elevation will be established at the time development plans are submitted.

Electrical and Natural Gas Systems

Pleasant Hill is served by Pacific Gas and Electric Company (PG&E) with both overhead and underground electrical facilities and underground gas facilities. There is adequate capacity for the projected development.

Existing facilities within easements or existing streets which are in conflict with the Specific Plans may need to be abandoned and relocated. Agreements between the city and PG&E will have to be made for these relocations and abandonments.

CHAPTER IV

URBAN DESIGN GUIDELINES

The Urban Design Guidelines control the relationship between various physical elements in the Downtown Plan environment to foster the creation of spaces which function effectively and are pleasant places for people. These guidelines translate the land use concepts and densities previously described into more specific and detailed instructions and requirements for the public, property owners, developers and staff to follow in implementing the plan.

The following guidelines are organized under the headings of Architectural Design, Circulation, and Gateway Design, Open Space, Landscaping, Signing, and Street Lighting.

ARCHITECTURAL DESIGN GUIDELINES

The purpose of the Architectural Design Guidelines is to ensure high quality architecture, visual consistency and appropriate form throughout the Pleasant Hill Downtown. Architecture of the highest quality is especially important given the variety of uses and buildings proposed in the mixed use development. The key elements to the design of the Downtown Plan are building massing, building form and composition, store fronts and materials, colors and finishes.

Building Massing

Building massing and height determine the overall presence and identity of the Downtown Plan. The majority of the area is proposed to be multiple stories. A uniform height will serve to strengthen the identity of the Downtown Plan and establish a consistent street frontage. This scale will provide a framework for developing the architectural themes of the downtown area.

Greater height and visibility are afforded the office buildings, hotel, and cinema complex because of their proximity to the freeway and the importance of establishing visual landmarks. The site placement of the office building and hotel frames the vistas to Crescent Street from the freeway. This view corridor will draw attention to, and be a reminder of, the presence of the Pleasant Hill Downtown to passing motorists. The greater height and massing along the freeway will also serve to reduce the impact of noise from this well-travelled corridor upon the several open-air environments contained in the Downtown Plan.

The following guidelines will assist in developing the proper scale.

- . Crescent Street retail development will be predominantly two stories in height.
- . Building masses up to five stories will be permitted on the east side of Contra Costa Boulevard provided development contains a mix of office and hotel uses.
- . Large building facades should be divided into smaller elements to develop an intimate scale. (The architecture of larger anchor tenant buildings may be considered for variance from this guideline.)
- . Second floor decks or terraces shall be encouraged to add sense of vitality to the streetscape.
- . Flat roofs with parapets are preferred. While other roof forms will be considered appropriate, residential type roofs are discouraged on commercial buildings.
- . Buildings shall be designed to enhance the street corners.

Building Form and Composition

The architecture of the Downtown should evoke a special feeling by providing an environment designed primarily for pedestrians and by creating a place which invites walking, shopping, and passive pursuits. The style of the architecture will evolve from consistent and carefully-proportioned use and interpretation of a vocabulary of architectural elements which could include but not limited to bay windows, balconies, awnings, arcades, staircases, domes, and towers.

The number of these architectural elements and the extent of their use in each area must be carefully evaluated, both as they relate to their immediate area and to the rest of the Pleasant Hill Downtown. Although variety is important, care must be taken to avoid a confusion of forms and the extremes of over-ornamentation or over-simplification. A careful balance of all design ingredients must be maintained.

The Downtown should not become a theme center designed with one architectural theme. Use of one style of form will quickly become dated and lose its excitement. The following guidelines are intended to provide for a long-term Downtown.

- . A single architectural theme or type should be discouraged.
- . Design forms should be integrated into a traditional downtown character. Simple, timeless design is encouraged.
- . Traditional commercial building forms such as overhangs, cornice and pilasters should be utilized.
- . Unique uses such as a cinema, health club, and hotel provide the opportunity to demonstrate architecture that relate to more classical styles.
- . A definitive architectural theme should be established for each zone which relates both to the qualities of the particular zone in which it is located and also to the qualities of the Downtown as a whole. The following are some qualities of the individual zones:
 - a) Crescent Street should use respective style elements with the major focus on the store fronts. There shall be design consistency along the length of the street.
 - b) Downtown Plaza should be an active area with open building facades which extend use into the square.
 - c) Office and/or hotel buildings on Area G provide the opportunity for freestanding buildings which could involve more contemporary architectural design and materials. These buildings, however, must be complementary with the character and materials utilized along Crescent Street.
 - d) Parking garage fronts shall be attractively designed as part of the Downtown.
 - e) Roof lines along Crescent Street should not appear continuous or give the impression of a large building mass. Roofs should break in form, slope or material.



DOWNTOWN PLAZA

FIGURE 13

PLEASANT HILL DOWNTOWN
CITY OF PLEASANT HILL, CALIFORNIA



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Store Fronts

The storefronts offer an area for greater attention to detail. Here is where proprietors establish their identity and announce or display their product. The customer should be able to perceive the nature of the product or service easily.

The design of the storefronts should allow variation of appearance without resulting in a confusion of effects or a lack of continuity. The individual store owner can have freedom of design working within an architectural framework without jeopardizing the overall character of the Downtown.

The basic storefront design guidelines encourage a mixture of store types and sizes as follows:

- . First floor frontages should have an integrated design including large display windows, entries and signing.
- . Entrances to second story office space should be distinct from store entries to avoid confusion and to establish identity. These are to have consistent form and provision for signing to clearly announce the nature of the use.
- . Depending upon the size of the store, they all should have the following common features:
 - a) Small stores (up to 2,000 s.f.)
 - single or double doors
 - zero or moderate recess at doors
 - 8 feet or less - soffit and doors
 - b) Medium stores (2-10,000 s.f.)
 - 1 pair doors
 - moderate recess at doors
 - 8 to 10 feet - soffit and doors
 - c) Larger stores (10-20,000 s.f.)
 - 2 pairs of doors
 - deeper recess at doors
 - 10 feet and over - soffit and doors



CONCEPTUAL ELEVATION ON CRESCENT STREET
AT ACCESS TO PARKING STRUCTURE

FIGURE 14A

P L E A S A N T H I L L D O W N T O W N
CITY OF PLEASANT HILL, CALIFORNIA



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CONCEPTUAL ELEVATION @ STREET INTERSECTION

FIGURE 14B

P L E A S A N T H I L L D O W N T O W N

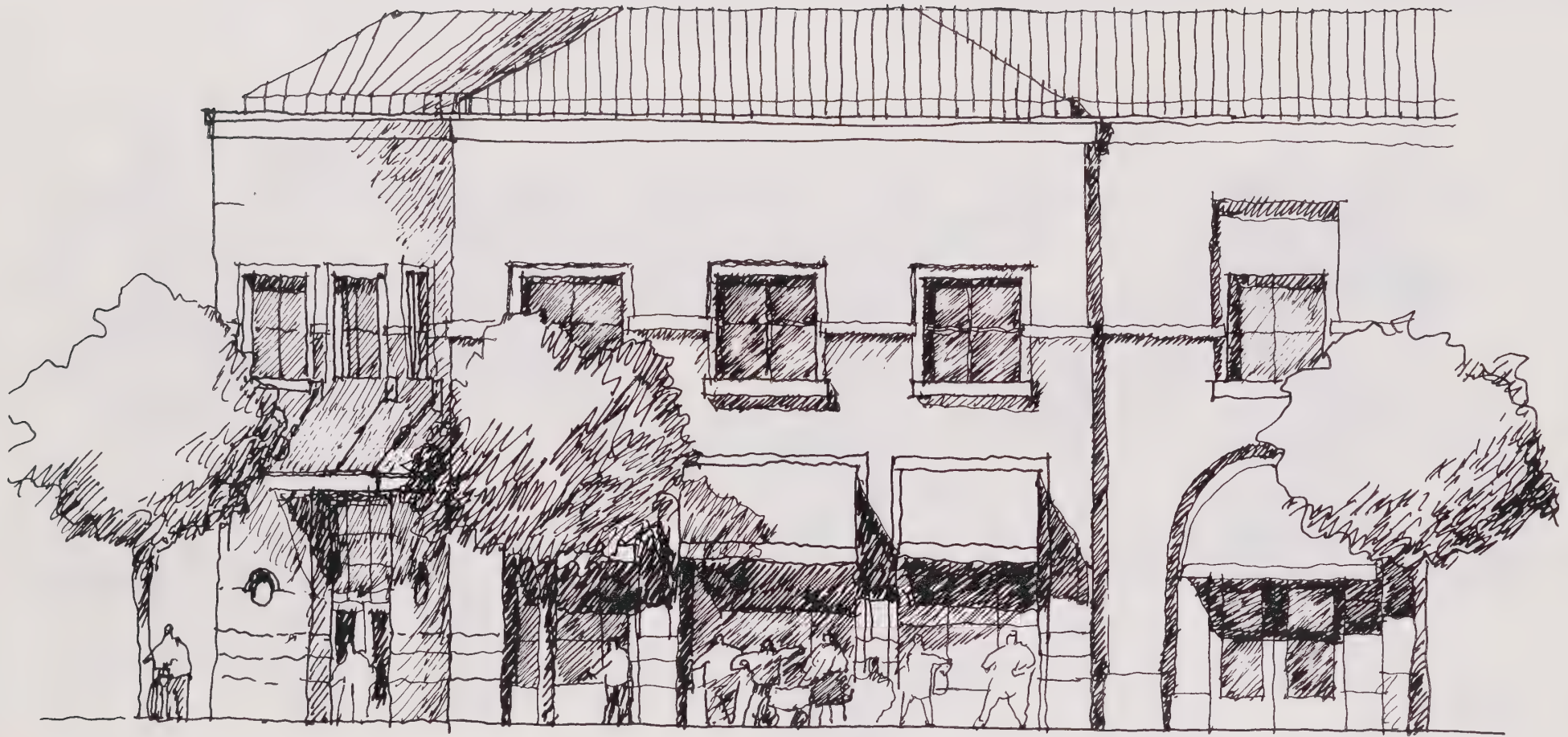
CITY OF PLEASANT HILL, CALIFORNIA



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CONCEPTUAL ELEVATION AT CITY HALL PASEO

FIGURE 14C



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CITY OF PLEASANT HILL, CALIFORNIA



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- . The building storefronts should be framed with a base, side columns or walls and a cornice overhang as other design feature.
- . Walls facing pedestrians ways are encouraged to have elements and visual interest, such as fenestration, displays, signing or landscaping.
- . Ground floor offices shall be required to maintain the same storefront character as retail spaces.
- . The use of awnings on building facades is encouraged to provide added color, weather protection and opportunities for signing.

Materials

All materials should be appropriate for their location considering the amount of physical contact, visual exposure and viewing distance. Color selections will provide a complementary variation of chroma to reduce linear expanse and to enhance the sense of human scale. Colors should be generally light to medium intensity. Primary colors should be reserved primarily for graphics. Glossy finishes and visually confusing patterns and combinations should be avoided.

The following are examples of types of materials appropriate for various purposes:

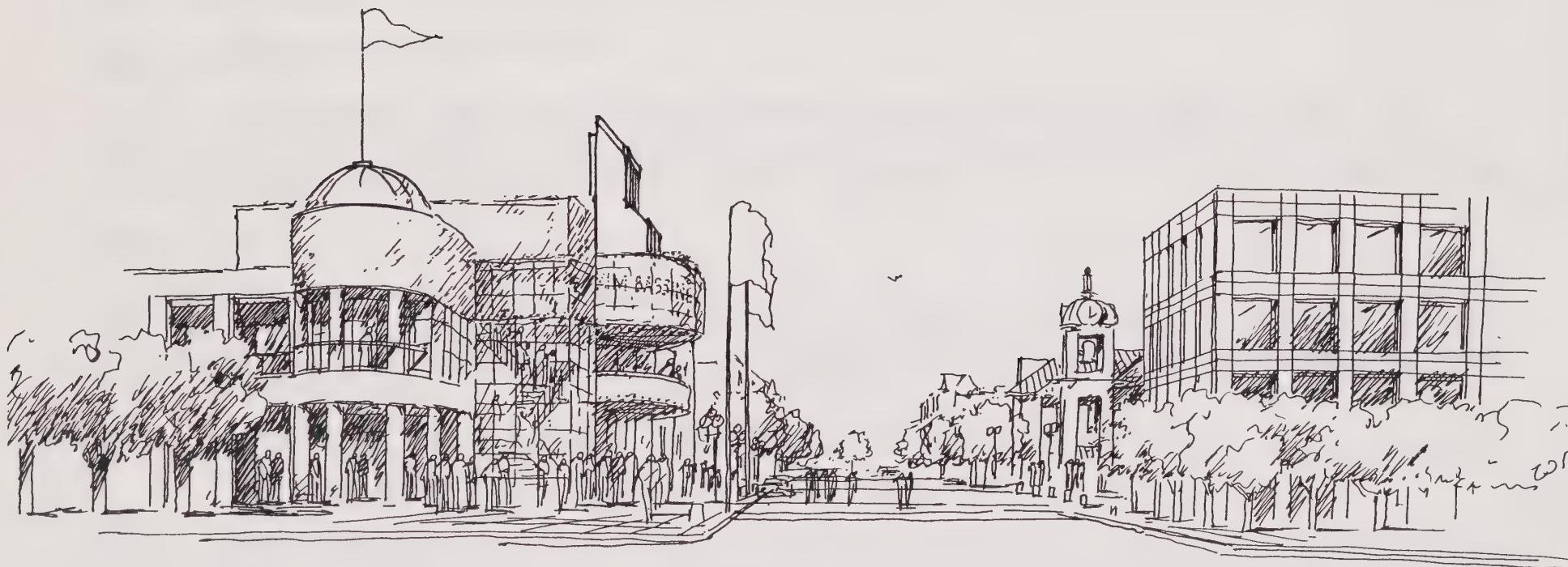
- . Retail areas with extensive public exposure and contact:
 - . Finished Stone
 - . Brick
 - . Concrete
 - . Steel
 - . Wrought Iron
 - . Tempered Glass
 - . Treated Wood
 - . Metal or tile roofing
 - . Tile
 - . Cement Plaster (painted)

- . Areas with moderate public exposure and contact such as office areas:
 - . Metal (painted or anodized)
 - . Glass
 - . Cement Plaster (painted)
 - . Fiberglass
- . Accent materials such as tile bases should be carefully chosen to reinforce the building style and coordinate with adjacent buildings.
- . Color schemes should be developed for each building which responds to the style of the structure and respects adjacent buildings.
- . Primary building colors shall be neutral tones to act as a background for trim, awning and signing.

Auxiliary Structures and Other Elements

At key locations, special structures shall be used as appropriate focal points or visual termination. Examples include:

- . Clock Tower in the Downtown Plaza - This element could be a kinetic focal point with a mechanical means of marking the passing of time. It could be one of the visual highlights of visiting the Pleasant Hill Downtown area and would be a fitting counterpoint to the activity within the plaza area.
- . Colonnades or Hemicycles - These could provide visual termination and punctuation at the ends of Crescent Street. Their presence should be balanced visually, with the surrounding paving, landscaping and architecture.
- . Trash collection and storage shall be carefully screened.
- . Mechanical equipment shall be screened from view.
- . Telephone booths, drinking fountains and newspaper displays shall be designed to integrate into the Downtown theme.



CONTRA COSTA BOULEVARD/MONUMENT PLAZA

FIGURE 15

P L E A S A N T H I L L D O W N T O W N

CITY OF PLEASANT HILL, CALIFORNIA



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CIRCULATION AND GATEWAY DESIGN GUIDELINES

The visual appearance of most downtowns is highly influenced by the circulation and roadway system. In many downtown areas, as much as a quarter of the total land area is utilized by streets, alleyways and service roads. The key elements to the design are entry elements, street widths, sidewalk widths, crossing points, parking, medians and landscaping. The following guidelines and drawings define the character of the major streets.

Gateways

Gateway intersections will provide that important first impression of arrival into the Downtown. Special design considerations should be given to enhancing the impression and guiding the users to their destinations. The guidelines for the Downtown Gateways are as follows:

- . Each corner should be readily identified with a unique design element.
- . Step back ground level of buildings and trees at street corners to maintain sight lines.
- . Provide a special identification statement for the Downtown through special signing or paving or landscaping at each gateway. Special efforts would be made to ensure that the design statement produce eye catching results to attract approaching vehicles and provide a positive image.

Streets

A significant amount of land is dedicated to streets in the Pleasant Hill Downtown Plan. Not only are the streets significant in land utilization, they provide a substantial feel to the environment. By designing an inappropriate roadway for the type of uses, an area can fail to achieve its intended goals. Therefore, the following guidelines intend to match the street design with land uses.

Contra Costa Boulevard

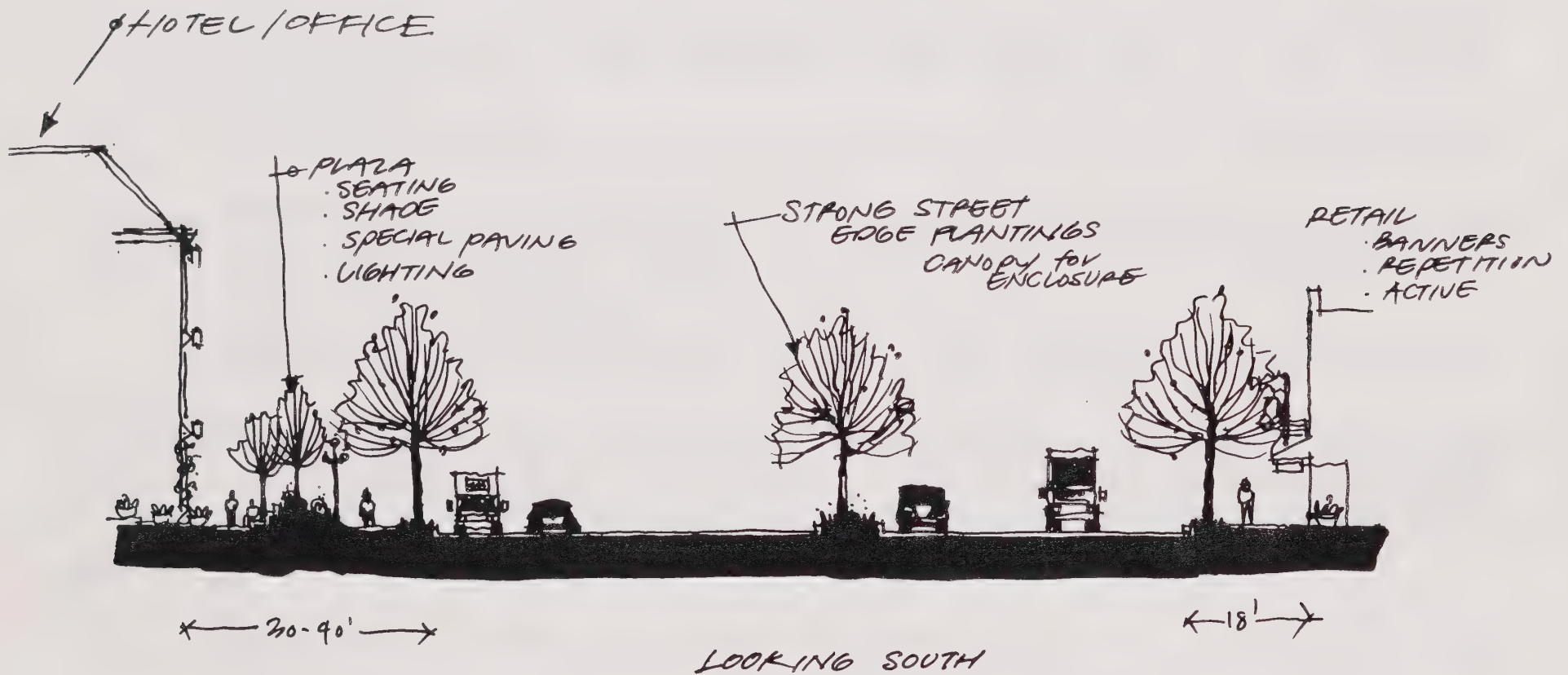
Contra Costa Boulevard is a downtown entrance street which provides visibility of the project and some access. It is crossed by Gregory Lane, Crescent Street and Monument Boulevard. A pedestrian bridge should be considered to provide crucial access to the future development on the eastern parcel. Shown as a principally auto-oriented street on the streetscape diagram, buildings along Contra Costa will be set back to allow room for landscaping strips with trees and shrubs. Some pedestrian amenities such as seating and special lighting fixtures should be provided. The general guidelines for Contra Costa Boulevard are as follows:

- . A pedestrian bridge or other special method of crossing Contra Costa Boulevard shall be considered.
- . Discourage pedestrian traffic along Contra Costa Boulevard by minimizing sidewalk width.
- . Limit access and turn movements from Contra Costa Boulevard.
- . Provide a well landscaped median.
- . Provide moderate setbacks from the curb face to evoke a more traditional suburban image.
- . Figure 16 illustrates the design concept for Contra Costa Boulevard.

Monument Plaza

Monument Plaza is one of the main gateways to the Downtown. Many activities and actions converge on to the street making it one of the most crucial design features in the Downtown area. Specifically, Monument Plaza serves as the main access street to the major parking garages, fronts onto the Downtown Plaza, shares a major intersection with Crescent Street and terminates as residential street. The following guidelines attempt to provide some cohesion to the street.

- . Buildings should be set back from 12-16 feet from the face of the curb from Contra Costa Boulevard to Crescent Street. Most of the setback should be sidewalk.
- . Small scale canopy trees should be planted in tree grates along the street.
- . Pedestrian drop-off turnouts shall be provided.
- . A planted median prohibiting left turn movements between Contra Costa Boulevard and Crescent Street shall be installed on Monument Plaza.
- . West of Crescent Street, Monument Plaza shall narrow to become a residential street.
- . Figure 18 illustrates the design concept for Monument Plaza.



CONTRA COSTA BOULEVARD

FIGURE 16

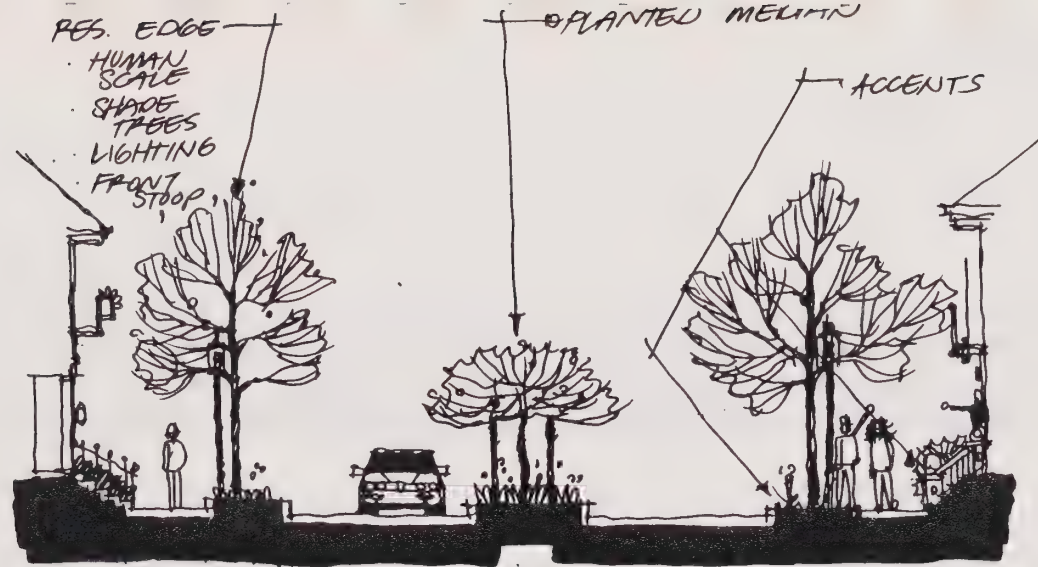
0 300 600 FEET

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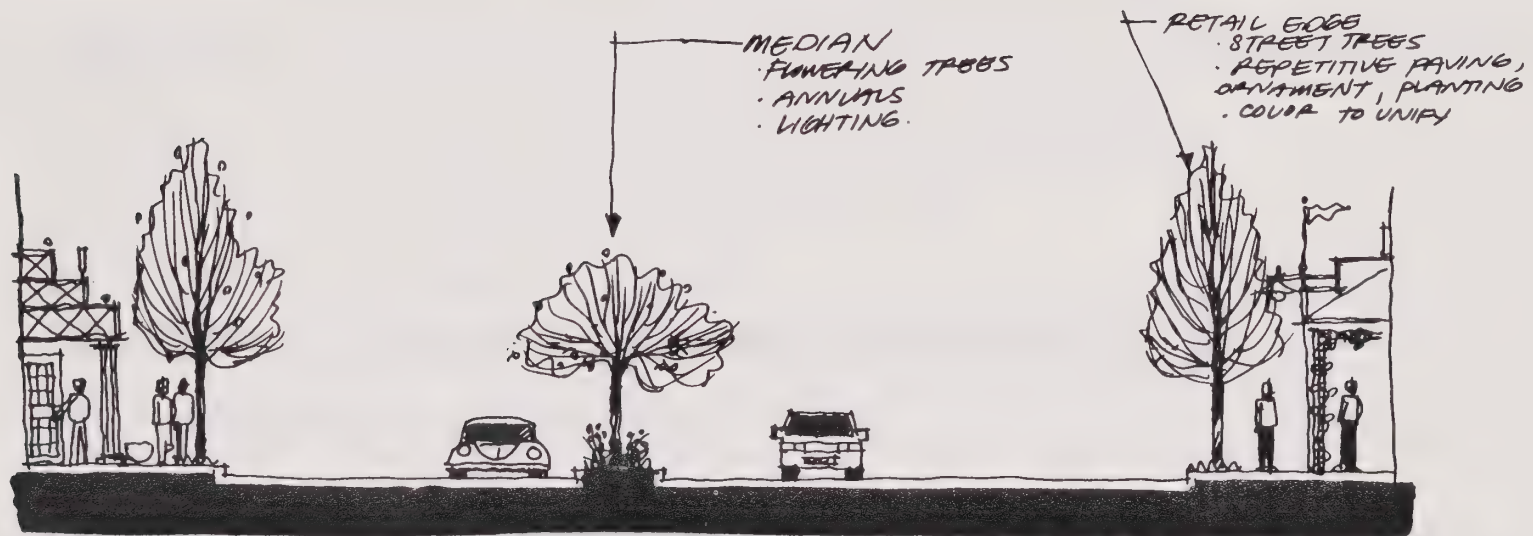
P L E A S A N T H I L L D O W N T O W N
 CITY OF PLEASANT HILL, CALIFORNIA



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MONUMENT @ RESIDENTIAL



MONUMENT at CRESCENT

MONUMENT PLAZA

FIGURE 17

0 300 900 FEET

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Crescent Street

Non-linear main streets are common to many older cities where neither the auto or topography dictate long straight streets. A crescent-shaped main street can permit a closed vista which reinforces the concept of a center, rather than a linear, limitless and vaguely comprehended place. A moving vantage point exposes the streetscape, buildings, signing, and landscaping in ever-changing images. This plan attempts to utilize the excitement of a main street to make the Downtown a unique place. The following guidelines enhance the visual experience.

- . Buildings should be set back as little as possible.
- . Banner poles, trees in tree grates, and special lighting features should line the wide sidewalks.
- . Diagonal parking will be provided on Crescent Street.
- . Crescent Street shall be as narrow as can be safely permitted.
- . Crosswalks should be emphasized with special planters, lighting and textured paving.
- . Create a pedestrian scale zone between buildings and the roadway that acts as a pleasing, comfortable environment by providing shade, sun pockets, seating areas, lighting, visual interest, texture and color.
- . Figures 6 and 18 illustrate the design concept for Crescent Street crossing and the view along the street.

Alleyways

Dedicated truck service and delivery lanes between the retail uses and garages, will provide service access to most retail space. This should limit on-street loading. The truck street will be designed to provide the opportunity to assist in the natural ventilation of the garages, while at the same time providing a landscaped and day-lighted void, spanned by pedestrian bridges to second level shops and offices and serviced with elevators and stairs to the retail street. Special guidelines are intended to insure that the alleys blend into the design. These are as follows:

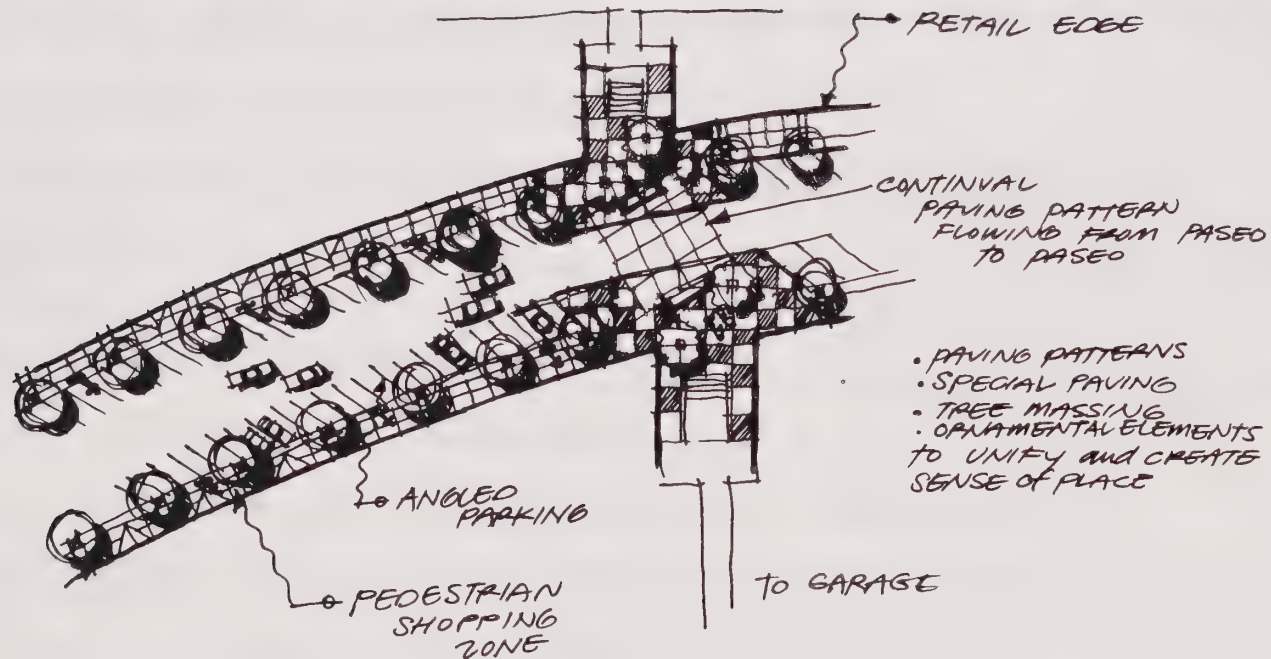
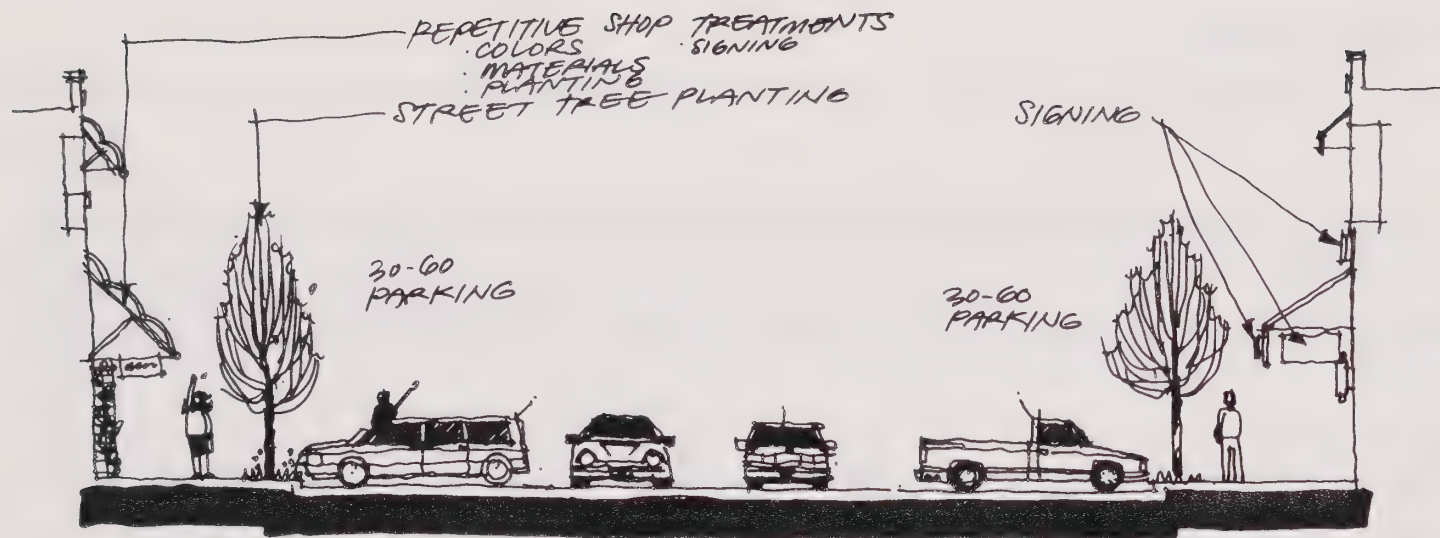
- . Provide an open air 20 ft. wide service alley between the rear of commercial buildings and the garage structure for service vehicle access. While the rear of the shops facing these passageways will be used primarily for service, they should be attractively maintained.
- . Locate trash collection and storage areas within structures rather than outside.
- . Design pedestrian-use areas to allow maintenance vehicles and emergency vehicles to pass.
- . Locate service alley entries close to automobile garage entries to minimize curb cuts.
- . Maintain a 13'-6" ft. vertical clear zone above service alleys.
- . Figure 19 illustrates the design concept for a typical alleyway.

OPEN SPACE GUIDELINES

The major unifying element in the design of the Downtown will be through inter-related open spaces.

The following guidelines will outline the relationship of open space in the Downtown area.

- . Make pedestrian movement paths within the Downtown interesting to the user through the modulation of open space and corridor spaces. Outdoor use areas should be organized into a hierarchical series of spaces or "outdoor rooms."
- . Screen unpleasant views while highlighting or creating pleasant views from paths. Framed vistas to Mt. Diablo and other key landmarks should be designed to provide users with an orienting framework and a sense of those elements which are unique to the Pleasant Hill Downtown.
- . Shade trees should be used to extend the outdoor use time by providing relief from the sun and softening extensive areas of building and paving.
- . Open spaces should provide a diversity of seating areas by providing low walls, benches and lawn areas.



CRESCENT STREET

FIGURE 18

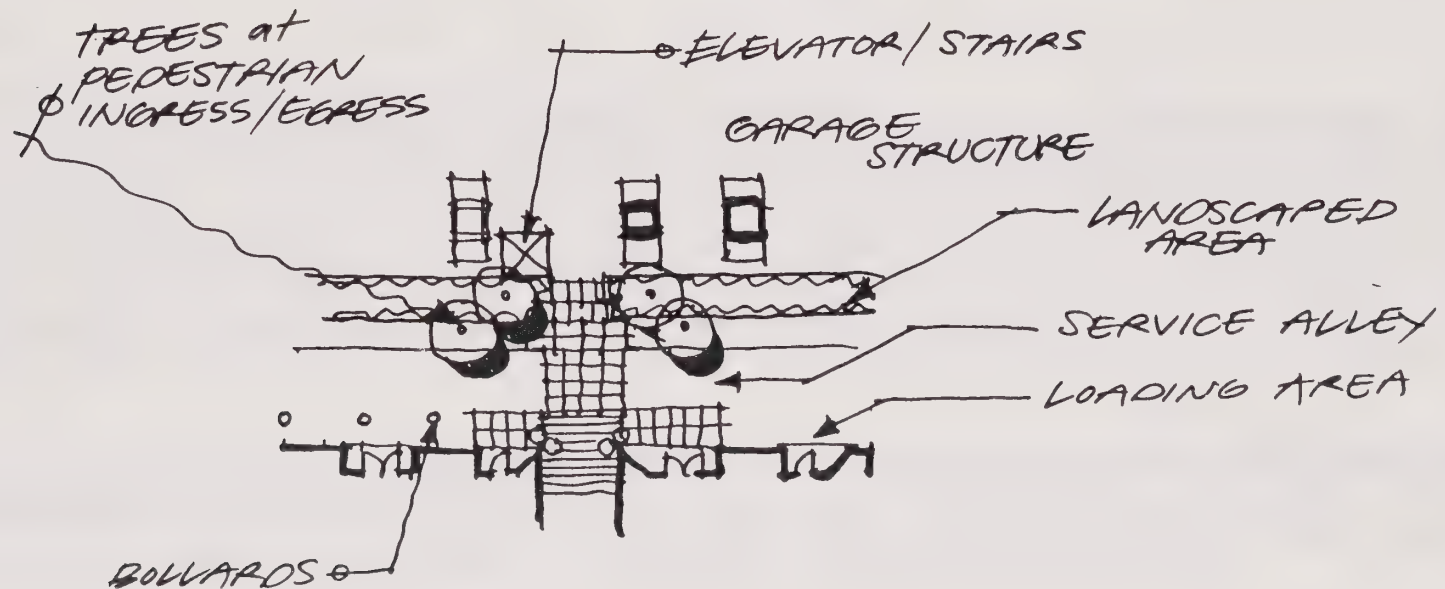
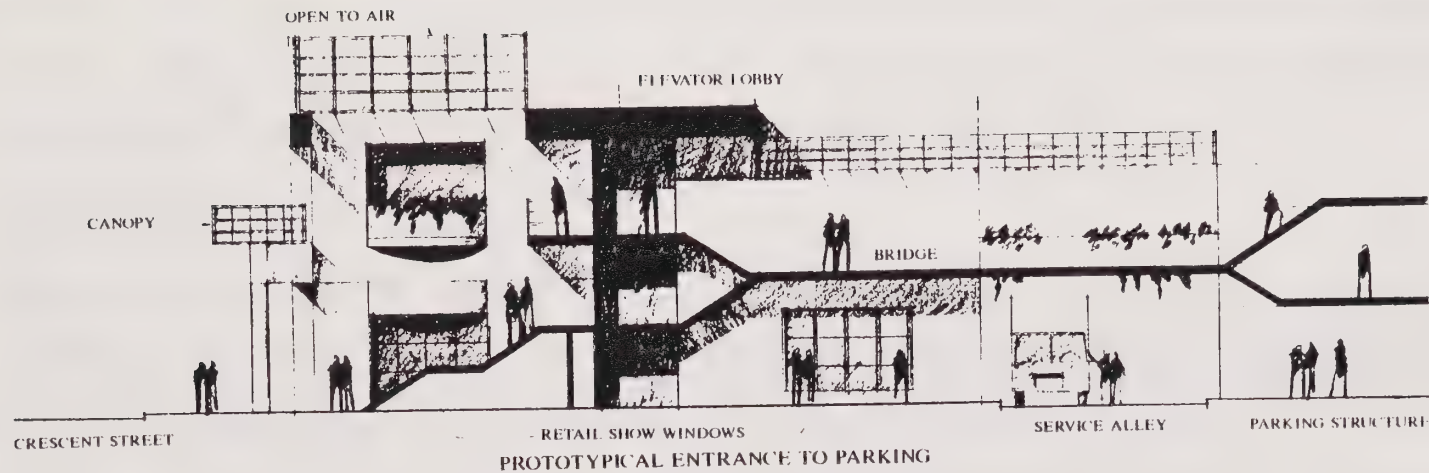
PLEASANT HILL DOWNTOWN
 CITY OF PLEASANT HILL, CALIFORNIA



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SERVICE ALLEY

FIGURE 19



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CITY OF PLEASANT HILL, CALIFORNIA



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- . Open spaces should provide relief from and contrast for the architecture surrounding it. Stepped-back buildings, overhangs, awnings, arcades or bosques of trees, shall be placed to reduce the scale of large buildings and create an active and interesting canopy for pedestrians.
- . Visual richness should be created through the use of special plantings, flowering trees, annuals, and a variety of furniture elements such as bollards, benches, pots and trash receptacles. Special urban landscape details such as tree grates, tree guards, carefully controlled lighting, sculpture, and water features shall also be used to add visual richness.

Downtown Plaza

The visitor's first impression of Downtown Pleasant Hill will most likely be influenced by the Downtown Plaza on Monument Plaza. This plaza will be the public pedestrian hub of the Downtown, an urban space devoted to the pedestrian only, entirely unimpeded by automobile traffic. Its character will convey both the festive nature and urban quality of the Downtown. Features planned for the plaza include a unique fountain, tree bosques, a clock tower and cafe seating. Active uses such as retail shops and restaurants, should form the boundaries for the plaza, resulting in increased foot traffic for businesses. The space could be used as the location for many activities, including a Farmer's Market, a lunchtime get-together at an outdoor restaurant, or post-movie people watching. The following guidelines will direct the refinement of the design:

- . Design the central plaza to act as a unifying element and focus for the surrounding building complex, as well as a public destination point that is sensed from the perimeters of the site.
- . Locate and design the plaza uses to respond to climatic patterns. Take advantage of sunny areas to create comfortable outdoor spaces. Building heights to the southwest of the plaza shall be constrained to two stories to allow sunlight to penetrate during lunchtime hours. This extends the outdoor use time for lunches, sunning, sitting and gathering.
- . Allow space for periodic public gatherings, performances, and festivities.
- . Enclose plaza on three sides with building masses.
- . Encourage outdoor food stands, flower stalls, and other vendors to utilize the plaza.

- . Provide a highly visible fountain focal element centrally located in plaza which does not interrupt pedestrian flow.
- . Provide a draw for people with lively activity; desirable smells and sounds.
- . Design spaces for people, with places to walk, sit, lean, and talk.
- . Provide ground-level interest at buildings with cafes and displays. Open-air dining is encouraged within the Downtown Plaza.

Paseo System

In the discussion of the overall circulation in the Downtown, in Chapter III, the paseo system was discussed in relation to its physical access requirements. The paseo system is more than a passage way between destinations, rather it has a special feeling of its own. Special care should be taken to ensure that design of the paseo system enhances the Downtown.

The guidelines for the paseo system are as follows:

- . The intensity of use of a pedestrian route shall determine path and paving materials. Major circulation routes shall have adequate room for two way foot traffic to pass comfortably, while allowing space for individuals to stop and linger momentarily.
- . Locate destinations and design the pedestrian circulation to eliminate "dead" corner.
- . Design circulation routes to be destination-oriented. Organize into a hierarchical system to facilitate understanding of the Downtown.
- . Provide environmental "clues" with the treatment of the circulation system: material, lighting, views, widths, and configurations to assist people with directions.
- . Water features such as pools, sprays, and fountains should be provided in the Downtown Plaza.
- . Signage should be encouraged for orientation, direction and visual interest.



DOWNTOWN PLAZA

FIGURE 20

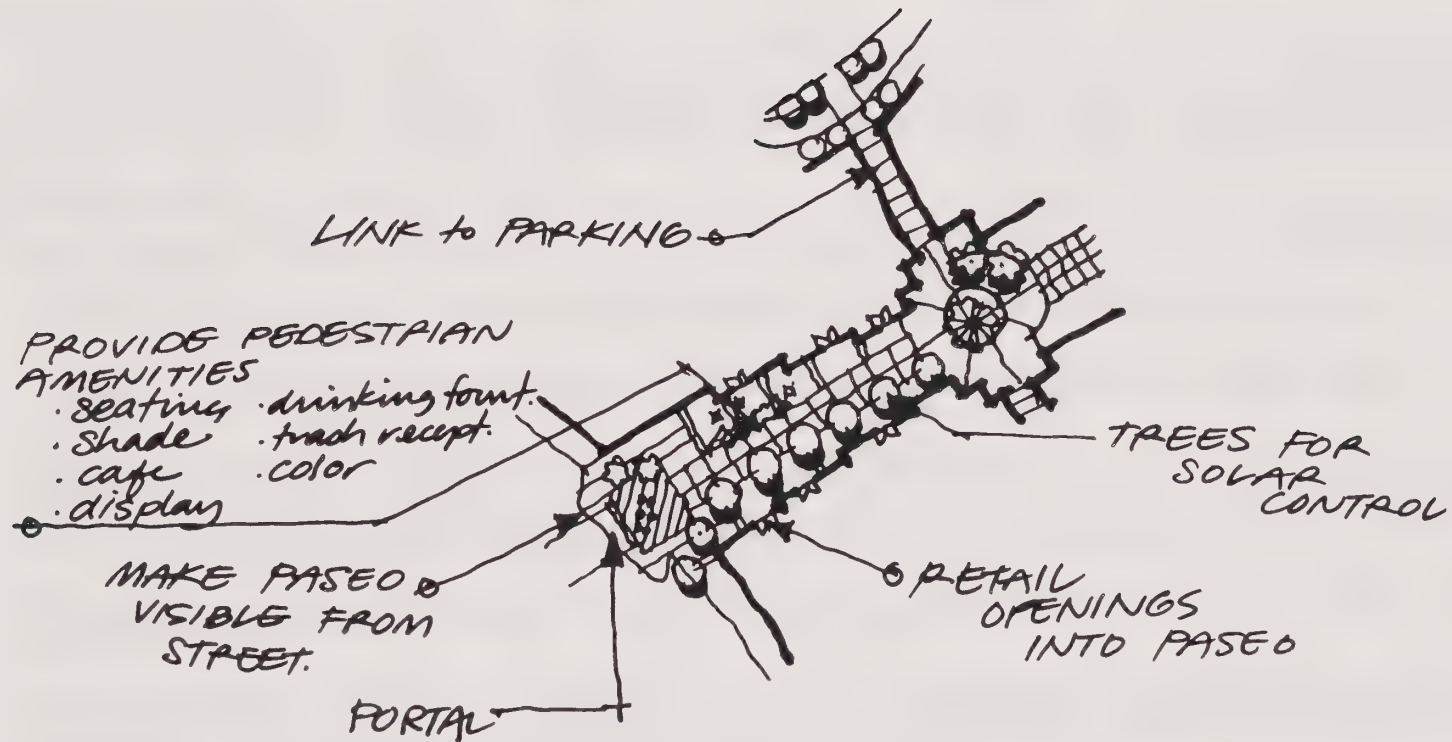
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PASEO SYSTEM

FIGURE 21



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P L E A S A N T H I L L D O W N T O W N
C I T Y O F P L E A S A N T H I L L , C A L I F O R N I A



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Arts

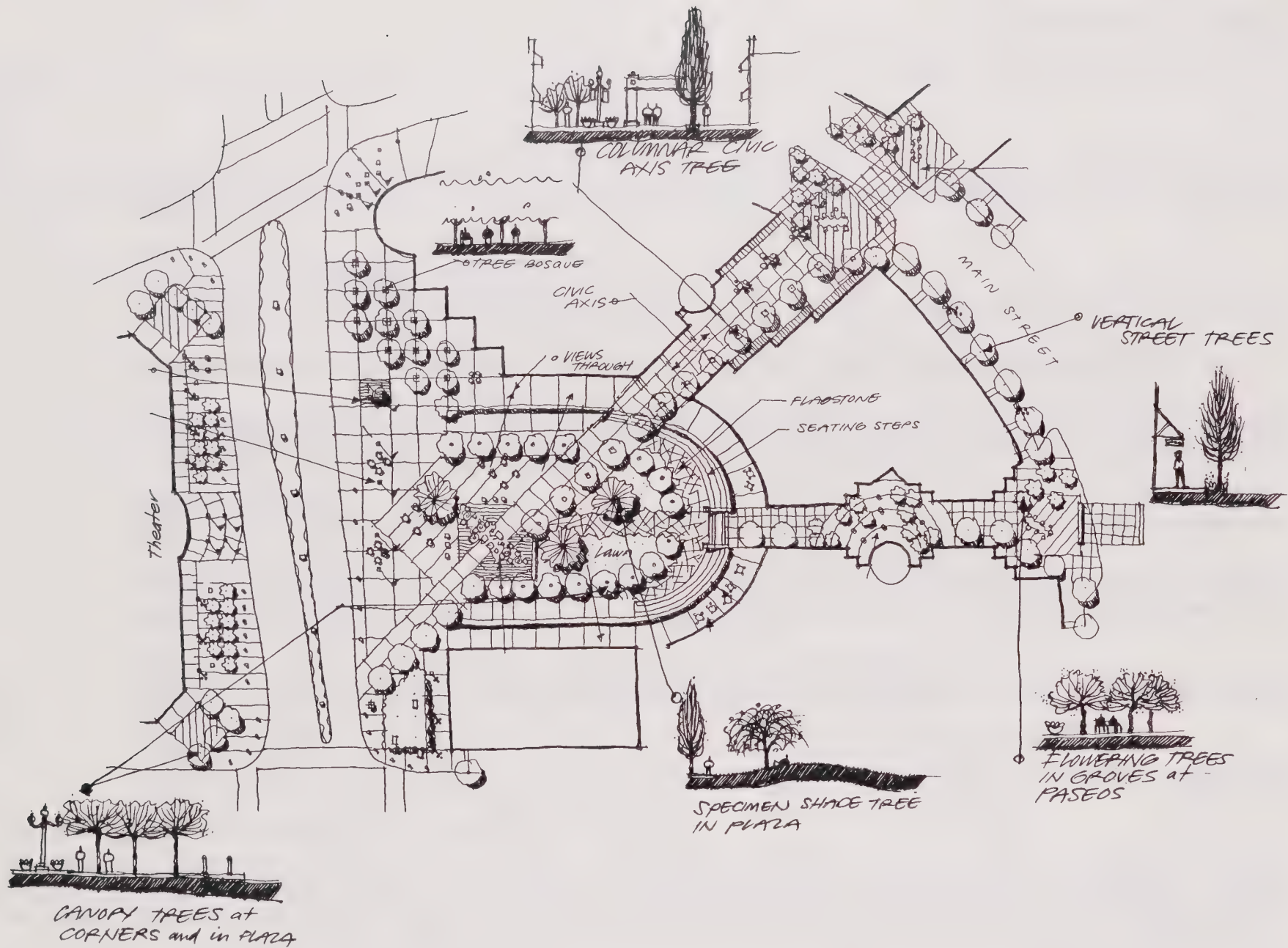
Art has always been a priority in the previous Commons Plans. The following are updated guidelines for the Downtown Plan which build upon the previous work.

- . Art shall be included in the development of major outdoor public spaces.
- . Pursue standards of excellence and recognition in terms of art and artists.
- . Pursue art which achieves popular acceptance and inspires lasting public acceptance, while not stifling artistic creativity.
- . Emphasize visually accessible art, which the community can understand and relate to; which invites contact and celebrates the "essence" of the Downtown; which is stimulating and inspirational; which enhances the urban experience.
- . Design art to be durable and compatible with the Downtown architecture.
- . Provide suitable setting for art work including landscaping and pedestrian amenities.
- . Encourage collaboration between artists, architects, and landscape architects.
- . Consider rotating art loans, temporary installations, as well as permanent acquisition of works of art.
- . Large scale major art work will be most effective located at points of high traffic and high visibility.
- . Works of art can range in scale from pieces large enough to be easily perceived as a focal point in the Downtown Plaza to a small pieces which can be suddenly "discovered" by the viewer entering smaller spaces.
- . Works of art should vary in material, form, scale, and character. Murals or mosaics might be designed into walls and paving. Lighting, planters, and other street furnishings can be designed in inventive, fanciful ways. Sculpture may be abstract, symbolic, or even express a "story." Art work can invite viewers' interaction with it by being sittable, climbable, readable, or even mobile.

LANDSCAPE GUIDELINES

As noted in the section on Street Design, a large percentage of the total land area is dedicated to areas without structures. As with the circulation system, landscape areas cover nearly one quarter of project site, thus making it an area that needs serious design treatments. The earlier section on Open Space Guidelines creates and organizes the unbuilt upon areas in which landscape will be installed. The provision of carefully selected and arranged landscaping will play a large role in emphasizing the "green" image of both Pleasant Hill in general and the Downtown development in particular. The following guidelines, along with the plant selection found in Appendix A, provide guidance for landscaping.

- . Design planting to express an overall concept. Dominant plantings should act as architectonic elements. Utilize plant materials in a logical, orderly manner to define spaces and to relate to buildings and structures.
- . Use plant materials in a way that responds to the need for solar control. Deciduous trees should generally be used on southern and western exposures to screen summer sun and permit winter light.
- . Select planting to relate to the scale of the intended viewer. For example, in areas where the primary viewer is the person in the automobile, large trees should be selected to respond to the scale of the road and the speed of movement.
- . Street trees and any trees installed in paved areas shall be placed within a root barrier to minimize future cracking and sidewalk upheaval.
- . Landscape structures should be incorporated into or adjacent to the buildings at appropriate locations that will not impede effective retail activity, including trellises, arbors, and planters.
- . Landscaping should be located to permit adequate sight distances for motorists and pedestrians at corners.
- . Healthy mature on-site trees should be preserved whenever possible. Ground disturbances and improvements under the drip lines of existing trees should be minimized.
- . Trees shall be planted from a 15 gallon size container, minimum. 50% of shrubs not used as a groundcover shall be planted from a 5 gallon container.



LANDSCAPE CONCEPT

FIGURE 22

0 300 900 FEET

PLEASANT HILL DOWNTOWN
CITY OF PLEASANT HILL, CALIFORNIA

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- . At planting, trees should have a minimum branching height of 10 feet above a road or parking surface, and 8 feet above paths. Trees should be located so as not to restrict movement along paths nor block signs.
- . Selection of plant materials should be based on year-round beauty (deciduous color, spring flower, fruit, branching pattern) as well as form, texture, shape, and ultimate growth.
- . Preferred streets trees shall be deep-rooted, canopy-type trees suited to urban conditions.
- . Tree placement must allow for sufficient root space adjacent to paved surfaces and underground utilities. Trees should be located 2 feet minimum away from curbs and sidewalks.
- . Select plants of the appropriate size for the intended use. Sensible initial plant selection will eliminate the need for high maintenance when the plants outgrow the space. (See Appendix A)
- . Use plant materials with deep non-invasive root systems and that will withstand smog and vandalism.
- . Achieve continuity of streets through use of repetition of similar trees and shrubs.
- . Choose trees with distinctive colors and textures for the Downtown Plan identity.
- . Lawn areas are not to exceed a 4:1 slope, and shrub areas are not to exceed a 3:1 slope.
- . Landscaping shall be carefully located and species selected so as not to impede views of landmark buildings and retail storefront signing.
- . Landscaped areas should be maintained in a healthy, thriving and weed free condition. Maintenance should include, where appropriate, pruning, mowing, weeding, cleaning, fertilizing and regular watering.
- . Plant materials shall be served by a city-approved automatic underground irrigation system. All backflow devices, controllers, and other exposed irrigation elements shall be screened from public view.
- . Irrigation systems shall be designed to minimize overspray into walks, parking lots, and other paved areas.

- . Irrigation system layout should acknowledge existing tree locations. Irrigation water shall not encroach within the dripline of existing trees which have been previously unwatered and are susceptible to water damage, e.g. native oaks.

SIGNING GUIDELINES

The primary purpose of general signing is to identify uses and offer public information. A well-designed signing system will not only provide information but also enhance the aesthetics and legibility of a building (or group of buildings).

To achieve this purpose the following guidelines shall be adhered to:

- . The primary purpose of the project sign system is to provide information and identification.
- . Each sign shall be designed to complement the architectural and landscape styles of the main building or buildings with respect to visual elements such as construction materials, color, or other design details.
- . Each sign shall take into consideration visual compatibility with the Downtown area.
- . The scale of signs, letters, and symbols should be appropriate to their use, whether to catch the eye of a passing motorists or strolling window shopper.
- . Color should be used carefully. Limited use of several colors with strong contrast between background and signing is recommended to make the signs easily readable.
- . Signs should be constructed with quality materials and in a craftsman-like manner to ensure both an attractive appearance and a durable project.
- . Public signing and graphics for traffic control and public information should be consistent throughout the Downtown. Special colors and consolidation of signs on special frames could add a positive element to the streetscape.
- . Promotional banners, balloons or similar promotional devices shall not be allowed, except when used to celebrate a specific event (including Grand Openings) and approved by the city.
- . Moving, flashing, or sound emitting signs are prohibited.

- . An effort shall be made to reduce copy down to the minimum necessary to convey the message.
- . Indirect sign illumination shall not cast a glare which will adversely impact any residential unit.
- . Temporary development and leasing signs shall be permitted during the development phase of the project for the purpose of identifying the business or company developing and leasing the parcel.
- . Exposed lamps or tubing, except neon, shall be discouraged.
- . All conduit, wiring, transformers, raceways and all fastening devices for sign, face, side, and exposed structures must be concealed from public area.
- . A special signing proposal for the Cinema use will be considered by the approving body. The proposal should show an architecturally integrated, urbane approach to color palette, materials, and forms. The allowable size shall be determined according to size, proportions, and setback of the building.

Business Signing

The intent of the business signing guidelines is to stimulate creative, good quality signing which will complement the scale and desired architectural character of the Downtown.

Because the Downtown will be almost completely new, there will not be a wealth of oversized, ugly signs to remove or replace. The guidelines are directed toward the future development. The guidelines are divided into three basic groupings: building mounted signs, retail storefront signs and detached business identification signs.

Building Mounted Business Signs

In traditional downtowns, older buildings were often identified with the name of the builder or proprietor and the date of construction. In its modern version, developments utilize parapet signs to identify the corporate identity. These signs should be used very judiciously.

- . Building mounted signs are permitted on buildings where a tenant is occupying the majority of one or more spaces, buildings or the entire sub-area.

- . Building mounted signs for all uses, except retail, cinema or recreational uses shall be limited to the occupant name or symbol.
- . No more than two building mounted signs will be permitted in any one sub-area without special review and approval.
- . Building mounted sign area is defined as the area of the surface or surfaces which displays letters or symbols identifying the occupant. Maximum sign area shall be set by the Architectural Review Commission.
- . The location of signs on buildings shall be immediately adjacent to, or above, the primary entrance, limited to the first 20 feet in height above street level, except for a maximum of two building identification signs at the parapet level of taller buildings. Signs may be mounted to vertical surfaces of a building or building-associated wall, provided such signs appear as an integral part of the overall architectural and site design concept. All signs shall be securely fastened to the building surface. Parapet level building mounted signs shall be discouraged on the west face of taller buildings.

Retail/Storefront Signs

The key to Downtown signing will be to take advantage of traditional sign forms. To do this, not only are awning and projecting signs allowed, they are encouraged. On Crescent Street and Monument Plaza awnings will be allowed to project into the right of way, provided a minimum vertical distance is maintained. The sidewalks will be of sufficient width that the projections would not have a detrimental effect on either pedestrians or vehicular circulation.

The specific requirements of the guidelines are detailed below.

- . Imaginative designs and placements of signing are encouraged to have an emphasis to pedestrians.
- . Sign information should be limited to shop's trade name, logo, and address.
- . Except for major anchor tenants, one storefront sign, window signing, and one projecting sign (blade or awning) will be permitted per storefront elevation. The following sign formats are acceptable:

1. Blade and Symbol Character Signs

Projecting non-illuminated blade and character signs will be encouraged, but limited to one (1) per storefront entrance and a maximum sign area of 6 square feet. All metal signs shall be backed with wood, even if wall-mounted. Blade signs shall be mounted a minimum of 8 feet clear above finish grade. Acceptable blade sign materials include:

- . Wood: carved, routed or turned.
- . Metal: painted, polished brass or chrome.
- . Wrought iron.
- . Tile.
- . Other materials, subject to approval.

Illuminated projecting blade and character signs shall be permitted on a site-specific basis, subject to approval by the Architectural Review Commission. Examples of acceptable illuminated sign materials include:

- . Neon.
- . Plexiglas.

2. Carved wood sign.

3. Three-dimensional backlit channel letters.

4. Internally illuminated cabinet sign.

5. Door/window sign: All signs or symbols applied to or suspended within the interior of the store which can be viewed from the street or sidewalk.

6. Neon sign.

7. Awning applied sign.

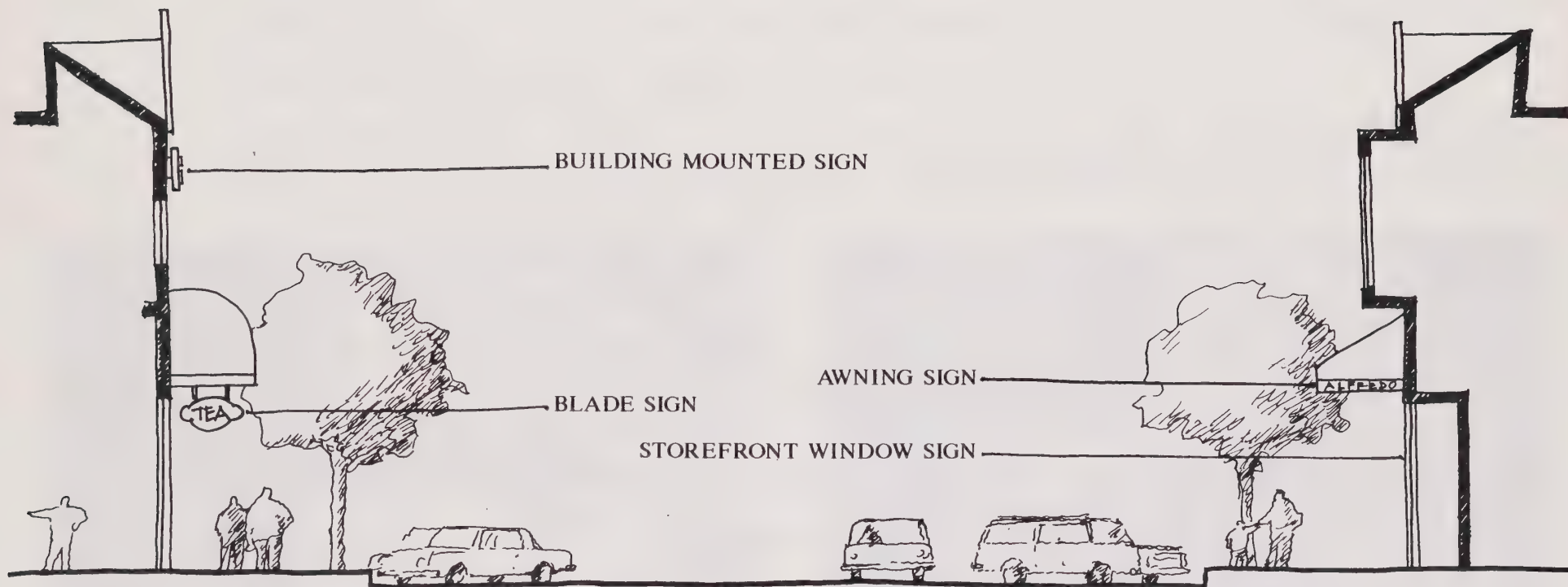
. Signing shall not exceed limitations specified in the city's Sign Ordinance.

- . The Redevelopment Agency's Graphics and Signing Policy (adopted 10-7-85) shall be the key reference in evaluating the appropriateness of sign proposals.
- . Signs shall be located on each retail commercial shop's storefront only.
- . Major anchor stores may utilize their usual identification signs on their buildings, provided, however, that there shall be no rooftop signs and signs which are moving, flashing or audible.

Detached Business Identification Signs

Detached signs (free-standing ground signs or monument signs) are not deemed appropriate in a downtown area and are discouraged in this Specific Plan. There are several locations or special installations in which these signs might be appropriate, for example along Contra Costa Boulevard. Specific requirements for detached signs are detailed below. Except as noted, the standard shall be those contained in the city's sign ordinance.

- . Detached business identification signs are permitted in auto-oriented areas where average building setbacks from the street exceed 25 feet.
- . Detached business identification signs shall be limited to the display of the name and logo of the building and the street address for the building.
- . No more than one detached business identification sign shall be permitted on each street frontage of a parcel.
- . No detached business identification sign shall exceed an area of 32 square feet per side. The sign area is defined as the area of the surfaces which display letters or symbols identifying the occupant of the parcel, or when the sign is of freestanding letters, the rectangular area which fully encloses all letters or symbols. The sign area shall not include the base or pedestal to which the sign is mounted.
- . All detached business identification signs shall be permanent ground-level signs and shall not exceed a height of six feet above the underlying finish grade.
- . Detached identification signs must be located within 20 feet of a fronting street and shall be located in such fashion as to provide maximum identity for the main entrance of any building occupied by the business so identified. Each



SIGN CONCEPTS

FIGURE 23

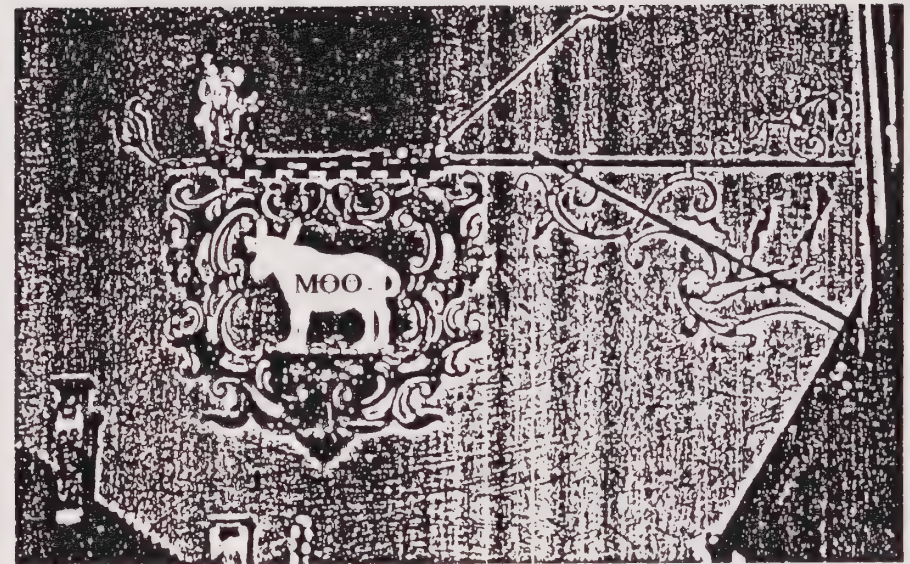
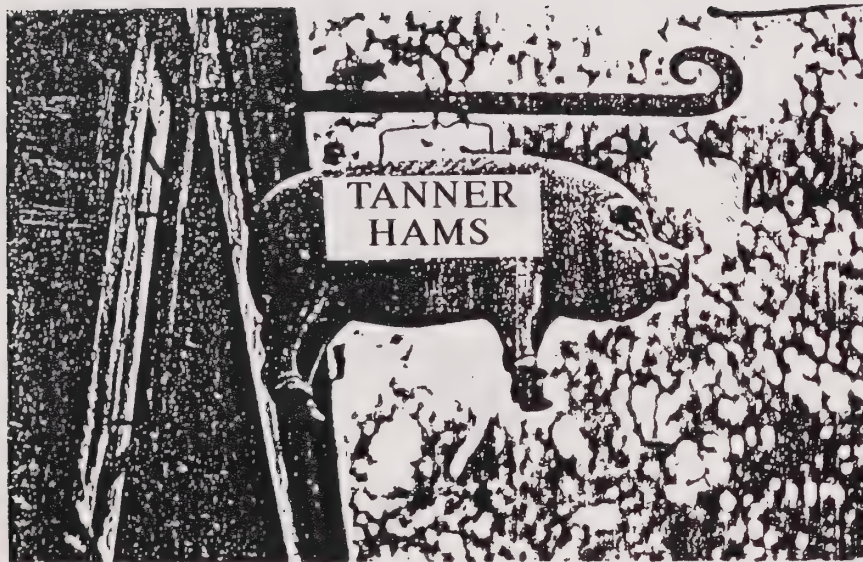
0 300 900 FEET

PLEASANT HILL DOWNTOWN
CITY OF PLEASANT HILL, CALIFORNIA



THE MARTIN GROUP
BORTOLASO AMERICA

HELLER & LEAKE ARCHITECTS
GATES & ASSOCIATES - LANDSCAPE ARCHITECTS



SIGN CONCEPTS

FIGURE 24

P L E A S A N T H I L L D O W N T O W N
CITY OF PLEASANT HILL, CALIFORNIA



THE MARTIN GROUP
BORTOLASO AMERICA



HELLER & LEAKE ARCHITECTS
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detached sign shall be located in a landscaped area which is of shape, design, and size that provides a compatible setting, ground definition to the sign, and an appropriate scale.

- . Signs shall be designed so as not to obstruct any pedestrian, bicyclist or driver's view of the street right-of-way.
- . Detached signing should be oriented perpendicular to approaching traffic.
- . Where there is more than one detached sign located upon a parcel, all such signs shall have designs that are complementary to each other, by similar treatment of type of construction materials, letter style of sign copy, support structure or method, and shape of the entire sign.

LIGHTING GUIDELINES

Site lighting should serve functional, safety, and aesthetic purposes. Light can be used to identify important civic buildings, thus giving cohesion to the physical structure of the community. It can be used to convey a private image for commercial or residential development, and it can be used to increase the use of public places at night.

The objectives of a lighting program within the Downtown, as developed by this Specific Plan, are to:

- . Encourage an abundance of high-quality outdoor lighting to give the Downtown vitality and sparkle at night.
- . Have on-site lighting contribute to site security.
- . Have on-site lighting complement and reinforce the architecture and parcel design character.
- . Have on-site parking area lighting fixtures and illumination levels be consistent throughout the Downtown Area.
- . Prevent on-site lighting from casting glare onto adjacent parcels and streets.
- . Encourage lighting design that is in conformance with energy saving guidelines.

To achieve these objectives, the following guidelines should be adhered to:

- . Light fixtures, functional only at night, are also viewed during the day, and therefore, must be integrated into the street furniture planning. The selection of special light fixtures for the Downtown will give cohesion to an area's visual image and identify it as separate from other neighborhoods and development in Pleasant Hill. Therefore, all public street and pedestrian lighting in the Downtown should be compatible in style, fixture color, and lamp color. The light fixtures will help to give the Downtown streetscape a unified appearance during the day and distinctive lighting at night. An overall lighting program for the Specific Plan Area will be developed.
- . The style of light fixtures and their location should complement the architectural and landscape design character. Imaginative lighting designs are encouraged.
- . Within the project, exterior lighting for pedestrian areas, building facades, and landscape design features should be complementary in style, color, and lamping with public street and pedestrian lighting. It is not necessary to duplicate light fixtures used on city streets but fixtures proposed for building exteriors must work effectively with streetlighting. Night lighting of buildings should be done in a selective fashion and shall be indirect in character (no light source visible). The following formats are among the acceptable means of architectural lighting:
 1. Keynoting special features such as towers and decorative cornices.
 2. Emphasizing repetitive elements such as columns.
 3. Using light to articulate architectural composition, such as spotlighting vertical elements of a vertical building, illuminating roof eaves.
 4. Using interior light sources as part of the total design. Architectural lighting should articulate and animate the particular building design.
- . Day and night lighting in garages should provide adequate illumination for safety and security of all areas, including corners, stairwells, entries, and ramps.
- . Light standard heights should be related to the lighting need of the use:

1. Street lights: up to 30 feet high
2. Parking areas: up to 18 feet high
3. Walkway and mall: up to 15 feet high
4. Planting areas: uplights or lowlights, up to 3 feet high.

- . Lighting for pedestrian movement should illuminate changes in grade, path intersections, seating area, and any other areas along a path which, left unlit, would cause the user to feel insecure. As a rule of thumb, one foot candle per square foot is adequate.
- . Building-mounted light fixtures are to be used judiciously. Their primary purpose shall be to illuminate pedestrian spaces. Subtle accent lighting of unique architectural elements may be considered. The arbitrary lighting of building facades and roofs shall be prohibited.
- . Light posts should be located in such a manner that they will not become safety hazards to pedestrians or vehicles.
- . Lights should not blink, flash or change intensity. Use of low pressure sodium lamps is not recommended due to poor color rendition.
- . Shatterproof or vandal resistant coverings are recommended for low-level lighting where there is danger of breakage.
- . Lighting systems shall be energy efficient.
- . Lighting should not intrude on adjacent property or glare into drivers' eyes. Any light source over 10 feet high should incorporate a cut-off shield to prevent light spill.
- . Service area lighting shall be contained within the service yard boundaries and enclosure walls. No light spillover should occur outside the service area. The light source shall not be visible from the street.

CHAPTER V

Land Use

The land use and development standards controlling the development in the Downtown Plan are contained in this Chapter. When taken as a unified set of regulations, this chapter contains those provisions that are included in the traditional zoning standards of the city's Zoning Ordinance. The specific standards in this Chapter govern over the guidelines found in other Chapters.

DEVELOPMENT REGULATIONS

The basic regulations that control the building height, bulk and setbacks are contained in Table 10. Although the Downtown Plan is envisioned to be developed as a single unified project, it is possible that development could occur incrementally with development occurring in each sub area developing independently. Because of this possibility, minimum lot areas and lot widths have been designated to ensure that development occurs in manageable increments.

Table 10

Development Regulations

Development Areas	A	B	C	D	E	F	G	H	Additional Regulations
Minimum Parcel Site* Area (acres)	2	3	3	3	3	3	8	2	
Minimum Parcel Site Width (ft.)	100	100	100	100	100	100	100	100	
Minimum Yards:									
Street (ft.)									
Contra Costa Blvd	-	5	5	-	-	5	20	-	(B)
Monument Blvd.	-	-	-	-	-	-	50	-	(B)
Monument Plaza	-	0	-	0	0	-	-	-	(B)
Crescent	-	0	0	0	0	0	-	-	(B)
Gregory Lane	-	-	-	-	-	20	-	20	(B)
Cleaveland Road	-	-	-	10	10	-	-	10	(B)
Boyd Road		10	0	-	-	-	-	-	(B)
North Main Street	10	-	-	-	-	-	-	-	(B)
Side (ft.)	50	0	0	0	0	0	50	0	(B)
Rear (ft.)	50	0	0	0	0	0	50	0	(B)
Maximum Height of Structures (ft.)	50	75	75	50	50	50	75	30	(C)(D)(E)
Maximum Height of Structures (Stories)	3	5	5	3	3	3	5	2	(E)

* "Parcel Site" refers to the aggregated land area to be developed as a single increment or phase of construction.

Development Areas	A	B	C	D	E	F	G	H	Additional Regulations
Maximum FAR**		.4	1	1	.6	.6	.6	.6	.3
Fences and Walls (Ft.)	-	-	-	-	-	-	-	-	(A)
Off-Street Parking and Loading	See Chapter 3 - Parking								(A)(F)
Vehicular Access	See Chapter 3 - Parking								(A)
Signs	See Chapter 4 - Signing								(A)
Outdoor Facilities	See Chapter 4 - Auxiliary Structures and Other Elements								(A)(G)
Mechanical Equipment	See Chapter 4 - Auxiliary Structures and Other Elements								(A)
Refuse Storage Areas	See Chapter 4 - Auxiliary Structures and Other Elements								(A)

** FAR (Floor Area Ratio) can be reallocated among areas A through F and H.

Additional Regulations

- A. See city Zoning Ordinance for method, definition and additional regulations.
- B. Projections into the required yard shall be permitted as follows:
 - 1. Cornices, eaves and ornamental features: 2 feet.
 - 2. Uncovered porches, terraces, platforms, decks, subterranean garages and patios: 3 feet.
 - 3. Balconies, stairs, canopies, elevators, awnings: 6 feet.
 - 4. Bay windows: 2.5 feet
 - 5. Signs
- C. Height shall be measured from average finished grade to an equal average height above.
- D. Towers, spirals, cupolas, chimneys, elevator penthouses, monuments, theater scenery, lofts, and similar structures and necessary mechanical appurtenances covering not more than 20 percent of the ground area covered by the structure to which they are accessory may exceed the maximum permitted height by 8 feet.
- E. Light, clock and theater marques shall be permitted to exceed the height, subject to approval by the Planning Commission.
- F. In development areas A and H, the parking requirements shall be those designated in the city Zoning Ordinance, unless a mixed use is proposed in those areas, if a mixed used is proposed, alternative standards will be established after a parking study by a registered Traffic Engineer has been conducted.
- G. Outdoor facilities: sidewalk cafes and outdoor food services accessory to a eating and drinking establishment shall be permitted subject to a use permit by the Zoning Administrator.

LAND USE CLASSIFICATIONS

This Specific Plan bridges the gap between the General Plan and Zoning. It also develops a new way of looking at land use regulations. The key to this approach is the use of the Land Use Classifications which describe uses of similar characteristics (the use classifications are found in Appendix B).

The Specific Plan and its accompanying Environmental Impact Report anticipated and evaluated the impacts of uses in each area based on a relative mix. Because of the mix that was as anticipated, several use classifications are considered appropriate, while others need to be evaluated because of traffic generation, parking demand or impact on adjacent uses. These later uses will require a use permit. As long as the amount and mix of uses correspond to those shown in Table 2, use permits would not be generally required.

The determination of whether a specific use shall be deemed to be within one or more use classifications or not within any classification shall be that of the Community Development Director. The Community Development Director may determine that a specific use shall not be deemed to be within a classification, whether or not named within the classification, if its characteristics are substantially incompatible with those typical of uses named within the classification. The decision the Director may be appealed to the Planning Commission and ultimately to the City Council, if necessary.

The following descriptions provide guidance on the types of uses anticipated with the classification. For further specificity, see Appendix B.

Mixed Use

Mixed uses can be found in many cities and town across the nation. Historically, they occurred naturally when shopkeepers lived above their shops. The flat, as well as the shop and the street were the merchant's home. Later, multiple units with shops below were common. Although the mix can be found today in many cities, mixed uses have also evolved in a different form. In suburban cities, mixed use could be found in a horizontal form in new "Planned Unit Development (PUD)." These were usually designed as large complexes on vacant land.

Some cities, both large and small, have tried to encourage mixed use through zoning by conditionally permitting commercial uses in residential areas or residential uses in commercial areas. This approach has been largely unsuccessful.

There are few examples for consciously planned mix use areas within an existing setting, largely because the need has only recently been acknowledged. The change from the automobile oriented single uses zoning of the post World War era has just begun.

In Pleasant Hill, mixed use, is permissive, in that the permitting process will be minimized. The historical combination of ground floor shops and flats will be allowed. Additionally, second and third floor office use will be allowed as permitted uses. To further encourage the mix of uses, areas with the mixed use designation will be permitted to have an overall floor area ratio of .75. Table 11 is provided to enumerate permitted land uses upon the sub areas in the Downtown Plan. Mixed use must include a combination of at least two of the following uses: retail, office, residential and commercial.

Retail

The key to the vitality of the Downtown is an active and lively retail. Retail uses includes a wide range of sales of merchandise to the general public. Within the retail classification, department stores, clothing stores, hobby shops, antiques, photographic supply, furniture and home improvement sales and others are permitted.

Office

The office designation corresponds to the traditional suburban automobile oriented single use. In Area A, offices would be allowed as the primary land use. The office designation encompasses general business operations, providing professional, executive, management administrative services. Other uses such as ground floor retail will be considered through a use permit.

Residential

Mixed use developments often include residential components. The Two Worlds project, on the north side of Gregory Lane, is a good example of how residential uses can co-exist and complement retail and office uses. Residential uses anticipated in this plan include townhouses, condominiums and apartments. Detached single family residential units and major residential care facilities are prohibited.

Public Facilities

The Specific Plan locates the City Hall in sub-area H of the Plan. The components of the Plan are based on the desire to create a vision of the future that complements the base retail environment and is integrated into the surrounding area. The City Hall builds on the vision that the key to a vibrant Downtown Plan is the mix of the various types of uses, thus Fire Stations, Parks, Civic buildings are all allowed by the Plan.

LAND USE REGULATIONS

Within this Specific Plan several areas will permit a combination of the various classifications, and are shown in Table 11, as being mixed use.

Table 11

Land Use Regulations

KEY:

P - Permitted	L - Limited, see Additional Use Regulations
U - Use Permit	-- Not Permitted

Areas	A	B	C	D	E	F	G	H	Additional
General Use	Office	Mixed	Mixed	Mixed	Mixed	Mixed	Mixed	Public	Regulations
Commercial Uses									
Adult Businesses	-	-	-	-	-	-	-	-	
Animal Sales and Services									
. Animal Boarding	U	U	U	U	U	U	-	-	
. Animal Clinic	U	U	U	U	U	U	-	-	
. Animal Grooming	U	U	U	U	U	U	-	-	
. Animal Hospitals	-	-	-	-	-	-	-	-	
. Animals Retail Sales	L1	L1	L1	L1	L1	L1	L1	-	
Artists' Studios	-	P	P	P	P	P	-	-	
Banks and Savings & Loans	P	P	P	P	P	P	L1	-	
. With Drive-up Service	-	-	-	-	-	-	-	-	
Commercial Filming	U	U	U	U	U	U	U	U	

Areas General Use	A Office	B Mixed	C Mixed	D Mixed	E Mixed	F Mixed	G Mixed	H Public	Additional Regulations
Commercial Recreation and Entertainment	L2	P	P	P	P	P	-	-	
Communication Facilities	U	U	U	U	U	U	P	-	
Eating & Drinking Establishments									
. With Wine and Beverage Service	L5	P	P	P	P	P	U/L3	-	
. With Live Entertainment	-	P	P	U	U	U	L3	-	
. With Take-out	U	U	U	U	U	U	-	-	
Establishments									
. Drive-up/Drive-thru	-	-	-	-	-	-	-	-	
. Food & Beverage Sales	L5	P	P	P	P	P	-	-	
Offices									
. Business/Professional	P	L4	L4	L4	L4	L4	P	P	
. Medical	P	P	P	P	P	P	U	U	(B)
Pawn Shops	-	-	-	-	-	-	-	-	
Personal Improvement Services	L5	P	P	P	P	P	L1	P	
Personal Services	P	P	P	P	P	P	L1	P	
Secondhand Appliance & Clothing sales	-	-	-	-	-	-	-	-	
Travel Services	P	P	P	P	P	P	P	P	
Hotels & Motels	-	P	P	P	P	P	P	-	
Eating & Drinking									

Areas	A	B	C	D	E	F	G	H	Additional
General Uses	Office	Mixed	Mixed	Mixed	Mixed	Mixed	Mixed	Public	Regulations
Residential Uses									
Day Care (limited)	-	-	-	P	P	P	-	-	(A)
Home Occupation	-	-	-	U	U	U	-	-	
Multifamily Residential	-	-	-	U	U	U	-	-	
Residential Care (limited)	-	-	-	U	U	U	-	-	
Public and Semipublic									
Clubs & Lodges	U	U	U	U	U	U	U	U	
Cultural Institutions	U	U	U	U	U	U	U	U	
Day Care, General	U	U	U	U	U	U	U	U	
Government Offices	L4	L4	L4	L4	L4	L4	L4	P	
Park & Recreation Facilities	-	-	P	U	U	U	U	P	
Religious Assembly	U	U	U	U	U	U	U	U	
Schools									
Public or Private	U	U	U	U	U	U	U	U	
Utilities, Major	U	U	U	U	U	U	U	U	
Utilities, Minor	P	P	P	P	P	P	P	P	
Temporary and Accessory Uses									
Arts and Crafts Shows	P	P	P	P	P	P	-	P	(A)
Christmas Tree Sales	-	-	-	-	-	-	-	-	(A)
Live Entertainment Events	U	P	P	P	P	P	U	P	(A)
Outdoor Exhibits	U	P	P	P	P	P	U	P	(A)
Recreational Events	P	P	P	P	P	P	U	P	(A)
Retail Sales, Outdoor	P	P	P	P	P	P	U	U	(A)
Street Fair	P	P	P	P	P	P	U	U	(A)

- L1 Permitted on the ground floor as part of a mixed-use project, occupying less than 25 percent of the gross floor area.
- L2 Only recreational courts and health/fitness club are permitted.
- L3 Allowed when part of a hotel.
- L4 Allowed on the second floor as a permitted use or on the ground floor with a Use Permit
- L5 Allowed when part of a health/fitness club
- (A) Subject to the conditions and criteria found in the city's Zoning Ordinance.
- (B) Subject to review and acceptance of off-street parking.

INTERIM USES

The Redevelopment Plan for Pleasant Hill Commons Redevelopment Area, allows uses that are not in conformity with the uses permitted in the Redevelopment Plan or zoning. This allows for the continued occupation and reoccupation of a building or land pending the final redevelopment.

In reviewing a request for an interim use, the city will consider the following:

- a) Prevailing economic conditions or redevelopment program priorities do not support immediate efforts to establish the uses designated in the Redevelopment Plan.
- b) The proposed interim use will not be an impediment to development of designated uses on abutting or nearby properties and the future development of the subject property.

To ensure that a proposed interim use does not impede the development of the Downtown Plan the standards as follows will be applied:

- 1) There is no increase in the gross floor area of any building on improved properties.
- 2) Unimproved properties shall be approved only for uses which do not require permanent structures.
- 3) All signing proposed in connection with the interim use shall be in conformance with the sign requirements in affect.
- 4) On-site parking, as required by the city, shall be provided and properly located for ease of access.
- 5) Perimeter landscaping shall be provided, selected from the plant materials list approved by the city.
- 6) The occupant/property owner shall enter into a Maintenance Agreement with the City of Pleasant Hill to ensure that the property is maintained at all times. Additionally a surety bond or other form of security shall be posed to ensure that when the use is vacated, the property shall be kept free of any debris as a result of the use.
- 7) Any use which requires any work done within the public right-of-way, an encroachment permit shall be obtained.
- 8) Surfaces shall be properly maintained to provide on-site drainage from the property.
- 9) A building permit shall be obtained for all interior work done to an existing building.

CHAPTER VI

IMPLEMENTING THE SPECIFIC PLAN

Realizing the goal of creating a Downtown for Pleasant Hill which uniquely adapts to the needs of the community will require a comprehensive strategy for implementing the Pleasant Hill Downtown Specific Plan. This means that a variety of planning efforts must be coordinated and directed toward similar objectives. Each planning tool that applies to land regulation, permitting, construction and financing must be utilized as it is most appropriate for the Downtown. Only through this approach can the Downtown grow swiftly, smoothly and in accordance with the desires of the community.

The two major options for regulating development are the Specific Plan and the Redevelopment Plan. Each of them have strengths in different steps of the implementation process. Adoption of the Specific Plan provides a set of guidelines for development and design. The Redevelopment Plan authorized the city to acquire and assemble land parcels for development, as well as collect an increment of tax revenues to provide funds for the capital improvements. Coordinating the two plans is essential to implementing the Specific Plan.

The Specific Plan should also be coordinated with the city's Zoning Code and permitting process. While Government Code 65455 allows Specific Plans to supersede zoning codes, making sure that these plans are consistent will eliminate unnecessary confusion among applicants and costly project delays.

The following provides greater detail on implementation procedures, including permitting and guidelines for compliance with the California Environmental Quality Act (CEQA). Additionally, a Public Facilities Plan, which identifies capital costs and spells out a strategy for financing the improvements, is discussed in the "Financing Public Improvements" section.

Permit Application Process

All development proposals in the Pleasant Hill Downtown Specific Plan areas are subject to review by the Architectural Review Commission and approved by the Planning Commission through the Planned Unit Development's requirement for a master use permit. If the proposal adheres to the provisions of the Specific Plan, the proposed project can proceed. If the applicant involves a divergence from the provisions of the Specific Plan, it becomes subject to more detailed study and scrutiny.

Inherent in the project approval process is consultation with the Community Development Department and design review. Project applicants should, as a first step, meet with a representative of the Community Development Department to discuss project concept plans, preliminary designs and the permit application process. Once a development application is submitted, the Community Development Department shall inform the applicant of an expected time schedule. The Department will then review the application for consistency with the Specific Plan and make findings on the determination. The application will then be forwarded to the Architectural Review Commission for a recommendation on consistency with the design guidelines established in Section IV of the Pleasant Hill Downtown Specific Plan.

Design review should be considered in the broadest possible terms. It should not be used only as an adjunct to improve "cosmetic" details. Rather, design review should address the entire range of design requirements involving the functions, visual and psychological fit of the proposed project in the overall environmental setting. In this way, architectural details are simply one aspect of the elements considered in the design review process.

Once the application has passed through the Community Development Department and the Architectural Review Commission, and findings of consistency with the Specific Plan have been made by both groups, it is forwarded to the Planning Commission. The Planning Commission then has the option to approve, conditionally approve, or deny the application.

Appeals to the Planning Commission's decision on an application may be taken to the City Council by the applicant or any other interested party. The appeal shall state specifically the claim of an error of discretion or the reasons why a decision is not supported by the evidence of the record. In considering such appeal, the City Council shall determine whether the proposed project conforms to the development and design review criteria, and may approve or disapprove the proposed project or require changes which are in its judgment necessary to ensure conformity to the criteria. The determination of the City Council shall be final.

California Environmental Quality Act

Commercial Projects

A focused Environmental Impact Report (FEIR) has been prepared which addresses the potential impacts of the non-residential land uses allowed by this Specific Plan. The FEIR identifies the impacts of the amount and mix of development described in the Specific Plan. If individually proposed projects are within this prescribed level of development, then the subsequent environmental review process should only address the project's site-specific impacts. If additional impacts are identified, an addendum EIR will be required.

Residential Projects

Pursuant to Section 65457 of the California Government Code, residential projects which are in conformity with the Specific Plan are exempt from subsequent environmental review, eliminating the need for additional EIR's. This exemption applies to all forms of residential projects.

Financing Public Improvements

The Downtown Specific Plan was recommended only after a careful consideration of the capacity required for roads and other public improvements that would be required to serve the area. Careful consideration was also given to the capacity of the City of Pleasant Hill to finance these public improvements. Land uses, public service capacity and financing capacity were all factors considered during the planning process. The results of this process and recommendations for financial implementation are described in the following paragraphs.

Redevelopment Tax Increment Financing

The California Community Development Law authorizes the financing of redevelopment projects through the use of tax allocation revenues. This method provides that the taxable valuation of the property within a redevelopment area on the last equalized assessment roll prior to the effective date of the adoption of the redevelopment plan becomes the base year assessment, and the increase in taxable valuation in subsequent years over the base year becomes the increment that may be allocated to the Redevelopment Agency. The Pleasant Hill Commons Redevelopment Project Area was established in 1974 and in that year the base assessment for the Commons area was \$14,139,500. By 1988, the base had increased to slightly over \$100,000,000, providing an annual income of over \$890,000.

For any given land use, an estimate can be made of the revenues likely to accrue from property taxes, sales taxes, and other sources of revenues. A number of assumptions are required, however, pertaining to the properties, site plan, construction costs, likely sites and likely income from hotel occupancy.

In Table No. 12, below, a rule of thumb valuation of different land use types has been outlined to determine the amount of bonding capability would be available to the city and Redevelopment Agency. The revenue from the tax increment can be converted to available funds through the Redevelopment Agency's bonding capacity. Bonding capacity refers to the amount of debt the Agency can issue.

Table 12
Bonding Capacity

Downtown Plan	
Property Tax	\$1,600,000
Less Low and Moderate Income Housing Set-Aside ¹	(320,000)
Less Non-Bonded Reserve (10%) (Includes portion of agency operation)	(160,000)
Equals Bondable Tax Increment	1,120,000
Divided by Coverage Factor	110%
Divided by Estimated Bond Debt Services Rate ²	7.5%
Equals Gross Bond Proceeds	13,600,000
Less Cost of Issuance (5% Max.)	<u>680,000</u>
Equals Net Bond Proceeds	\$12,920,000

¹ By law, 20 percent of tax increments accruing to redevelopment agencies from project areas adopted after January 1977 is to be reserved for construction of low and moderate income housing stock. This provision applies to the entire Schoolyard redevelopment area but only a small portion of the Commons area added after January 1977. The amount attributable to the Commons area is negligible and is omitted from this analysis.

² Assumes bonds would be exempt from federal and state taxation. Taxable bonds would likely carry a higher interest rate, yielding lower bond proceeds.

The available bond proceeds can be used to finance capital projects and such as street-widening, public open space development, parking structures. Additionally, these funds can be used for acquisition of land and relocation of existing uses.

Dedication and Development Fees

Development in the Downtown Plan areas is subject to all the changes and development impact fees normally levied by the City of Pleasant Hill, the utility districts, fire district and the Mount Diablo School District. The fees, particularly the city Traffic Mitigation fee can offset some of the anticipated costs for road improvement.

Based on the projected development in the Downtown area, the city can expect approximately \$4,550,000 in traffic mitigation fees. These funds should be designated in separate accounts for use on road projects within the boundary of the Downtown. Consideration should be given to providing credits to the developers in the Downtown for improvements for regional facilities.

Community Facilities Districts

The Mello Roos Community Facilities Act of 1982 was enacted to aid in the financing essential public facilities which normally accompany major development projects. The act is similar to an assessment district with several notable exceptions. Most significantly a Mello Roos District can finance any tangible or real property with an extended life of 5 years or greater to be owned or operated by a public agency plus any fees to pay for such capital facilities. Assessment districts can be formed only for public facilities whose benefits are identifiable and whose costs can be easily apportioned among the properties.

A Mello Roos district provides a special tax levied on an annual basis. The revenues may be used to directly supply qualified services to the area, such as the maintenance of parking garages or open areas, or they may be pledged to support debt services on bonds issued to finance public facilities such as the parking garages.

To determine the amount of debt, a valuation of the land is necessary. After an estimate of the market value has been prepared, consideration should be given to issuance of bonded debt, the face amount of which is not greater than one-third the appraised market value. In other words, bond buyers can look to the land that has a marked value at least three times as high as the total amount of issued debt.

A ratio of 3:1, value to debt, is by no means an iron-clad rule in California. None the less, at least initially it represents a conservative and prudent limit to the amount of debt that would be issued. Generally, a Mello Roos District is structured to occur bonded indebtedness for the entire construction life of a phased project yet only issue as much debt as is needed to finance public improvement for each particular phase. As an early phase is built out, the district has the authority to issue additional bonds to finance public improvements for subsequent phases of the project; a type of "pay as you grow" program.

It is likely that the Downtown Plan will be developed in several phases because of market constraints. The Mello Roos funding source is particularly attractive in this type of project for improvements that could be phased, such as the parking structures.

Implementation Program

At the time of the adoption of the Specific Plan, the process of reconciling the financial requirements of the Downtown Plan project cannot be determined. The final financing program will most likely be a combination of the methods outlined above and determined through negotiations with the developer or developers of the project. The specifics of the program will be part of any future Development and Disposition Agreements or Owner Participation Agreements entered into with the Pleasant Hill Redevelopment Agency and developers.

Factors that the city should consider when selecting the techniques of financing for individual improvements include:

- . Required timing of the improvements compared to location and rate of development
- . Feasibility of constructing an improvement on a "pay as you go" basis compared to "all at once" construction (requiring "all at once" financing).
- . Feasibility of simultaneous processing of several development applications to assure an adequate basis for financing major improvements.
- . Capacity to coordinate the timing of approval of individual bond authorities and construction requirements and maintain compliance with then-applicable requirements for tax exempt financing.

Preference of the master developer or individual landowners should be balanced with the overall requirements for efficient and equitable implementation of the Capital Improvement Program for the Downtown.

APPENDIX A

The following plant list represents a partial sampling of trees, shrubs, groundcovers, and vines suitable for use in the Pleasant Hill Downtown Plan. Further refinement in the selection of plant materials should be a function of specific site design.

Street Trees

<i>Celtis sinensis</i>	Chinese Hackberry
<i>Fraxinus oxycarpa</i> "Raywood"	Raywood Ash
<i>Fraxinus uhdei</i>	Evergreen Ash
<i>Geijera parviflora</i>	Australian Willow
<i>Gleditsia tricanthos inermis</i>	Thornless Honey Locust
<i>Ligustrum lucidum</i>	Glossy Privet
<i>Liriodendron tulipifera</i>	Tulip Tree
<i>Platanus acerifolia</i>	London Plane Tree
<i>Pyrus calleryana</i>	Bradford Pear
<i>Quercus palustris</i>	Pin Oak
<i>Quercus rubra</i>	Red Oak
<i>Quercus virginiana</i>	Southern Live Oak
<i>Tilia cordata</i>	Little-leaf Linden

Median/Accent Trees

<i>Crataegus phaenopyrum</i>	Washington Thorn
<i>Lagerstroemia indica</i>	Crepe Myrtle
<i>Malus species</i>	Crabapple
<i>Nerium oleander</i> , standard	Oleander (Standard Form)
<i>Pistacia chinensis</i>	Chinese Pistache
<i>Pyrus kawakamii</i>	Evergreen Pear

Shrubs

Abelia grandiflora	Abelia
Agapanthus species	Lily-of-the-Nile
Cotoneaster species	Cotoneaster
Coprosma kirkii	Creeping Coprosma
Escallonia "Terri"	Escallonia
Euryops pectinatus	Euryops
Hebe species	Hebe
Hemerocallis hybridus	Daylily
Myrtus communis "Compacta"	Compact Myrtle
Nandina domestica	Heavenly Bamboo
Nerium oleander	Oleander
Pittosporum tobira	Pittosporum
Raphiolepis indica	Indian Hawthorn
Rosmarinus officinalis	Rosemary
Sollya heterophylla	Australian Bluebell Creeper
Viburnum tinus	Viburnum

Groundcovers

Fragaria chiloensis	Sand Strawberry
Gazania species	Gazania
Hypericum calycinum	Aaron's Beard
Hedera helix	English Ivy
Myoporum parvifolium	Creeping Myoporum
Trachelospermum jasminoides	Star Jasmine
Vinca minor	Dwarf Periwinkle

Vines

Clytostoma callistegiodes
Ficus pumila
Hardenbergia violacea
Jasminum polyanthum
Parthenocissus tricuspidata
Rosa banksiae
Solanum jasminoides
Wisteria sinensis

Violet Trumpet Vine
Creeping Fig
Lilac Vine
Jasmine
Boston Ivy
Lady Banks Rose
Potato Vine
Wisteria

APPENDIX B

Residential Use Classifications

- A. Day Care, Limited. Non-medical care and supervision of six or fewer persons on a less than 24-hour basis. This classification includes day-care centers for children and adults.
- B. Home Occupation. Occupations engaged in solely by the occupant of a dwelling unit which are conducted entirely within a portion of the principal dwelling unit in a residential district that are incidental to the principal residential use of a lot.
- C. Multifamily Residential. Two or more dwelling units on a site. This classification includes mobile home and factory-built housing.
- D. Residential Care, Limited. Twenty-four-hour non-medical care for six or fewer persons in need of personal services, supervision, protection, or assistance essential for sustaining the activities of daily living. This classification includes only those services and facilities licensed by the State of California.

Commercial Use Classifications

- A. Adult Businesses. Establishments based primarily on materials or performances that depict, describe, or relate to "specified sexual activities", as defined in the Municipal Code.
- C. Animal Sales and Services.
 - 1. Animal Boarding. Provision of shelter and care for small animals on a commercial basis. This classification includes activities such as feeding, exercising, grooming, and incidental medical care.
 - 2. Animal Clinics. Facilities which provide grooming, training or other services to animals, including veterinary services on an outpatient basis with no overnight boarding.

3. Animal Grooming. Provision of bathing and trimming services for small animals on a commercial basis. This classification includes boarding of domestic animals for a maximum period of 48 hours.
 4. Animal Hospitals. Establishments where small animals receive medical and surgical treatment. This classification includes only facilities that are entirely enclosed, soundproofed, and air-conditioned. Grooming and temporary (30 days) boarding of animals is included if incidental to the hospital use, and limited to animals receiving medical care within the main building or within a detached secondary building.
 5. Animals: Retail Sales. Retail sales and boarding of small animals, provided such activities take place within an entirely enclosed building. This classification includes grooming if incidental to the retail use.
- D. Artists' Studios. Work space for artists and artisans, including individuals practicing one of the fine arts or performing arts, or skilled in an applied art or craft.
- E. Banks and Savings and Loans. Financial institutions that provide retail banking services to individuals and businesses. This classification includes only those institutions engaged in the on-site circulation of cash money. It also includes businesses offering check-cashing facilities.
1. With Drive-up Service. Institutions providing services accessible to persons who remain in their automobiles.
- F. Commercial Filming. Commercial motion picture or video photography at the same location more than six days per quarter of a calendar year.
- G. Commercial Recreation and Entertainment. Provision of participant or spectator recreation or entertainment. This classification includes theaters, sports stadiums and arenas, amusement parks, bingo game parlors, bowling alleys, billiard parlors, poolrooms, dance halls, ice/roller skating rinks, and archery ranges, scale-model courses, tennis/racquetball courts, croquet courts, swim clubs, health/fitness clubs, pinball arcades or electronic games centers having more than five coin-operated game machines, and card rooms, and the like.
- H. Communications Facilities. Broadcasting, recording, and other communication services accomplished through electronic or telephonic mechanisms, but excluding Utilities (Major). This classification includes radio, television, or recording studios; telephone switching centers; and telegraph offices.

- K. Eating and Drinking Establishments. Businesses serving prepared food or beverages for consumption on or off the premises.
 - 1. Bars With Live Entertainment/Dancing.
 - 2. With Take-Out Service. Establishments at which 20 percent or more of the transactions are sales for off-site consumption.
 - a. Drive-through. Service from a building to persons in vehicles through an outdoor service window.
- J. Food and Beverage Sales. Retail sales of food and beverages for off-site preparation and consumption. Typical uses include groceries, liquor stores, or delicatessens. Establishments at which 20 percent or more of the transactions are sales of prepared food for on-site or take-out consumption shall be classified as Catering Services or Eating and Drinking Establishments.
- K. Offices, Business and Professional. Offices of firms or organizations providing professional, executive, management, or administrative services, such as architectural, computer software consulting, data management, engineering, interior design, graphic design, real estate, insurance, investment, legal, and medical/dental offices. This classification includes medical/dental laboratories incidental to an office use, but excludes banks and savings and loan associations.
 - 1. Offices, Medical. Offices for a physician, or dentist., or chiropractor.
- L. Pawn Shops. Establishments engaged in the buying or selling of new or secondhand merchandise and offering loans secured by personal property.
- M. Personal Improvement Services. Provision of instructional services or facilities, including photography, fine arts, crafts, dance or music studios, driving schools, business and trade schools, and diet centers, reducing salons, and fitness studios.
- N. Personal Services. Provision of recurrently needed services of a personal nature. This classification includes barber and beauty shops, massage, tanning, seamstresses, tailors, shoe repair shops, dry cleaning agencies (excluding plants), photo-copying, and self-service laundries., and the like.

1. Limited. Excludes dry cleaning agencies and self-service laundries.
- O. Retail Sales. Including, but not limited to, the retail sale of merchandise not specifically listed under another use classification. This classification includes department stores, clothing stores, and furniture stores, and businesses retailing the following goods: toys, hobby materials, handcrafted items, jewelry, cameras, photographic supplies, electronic equipment, records, sporting goods, kitchen utensils, hardware, appliances, art, antiques, art supplies and services, paint and wallpaper carpeting and floor covering, office supplies, bicycles, and new automotive parts and accessories (excluding service and installation).
1. Limited. Excludes furniture, hardware, paint and wallpaper, carpeting and floor covering, and new automotive parts and accessories.
 2. Pharmacies and Medical Supplies. Establishments primarily selling prescription drugs, medical supplies, and equipment.
- P. Secondhand Appliance and Clothing Sales. The retail sale of used appliances and clothing by secondhand dealers. This classification excludes antique shops primarily engaged in the sale of used furniture and accessories other than appliances.
- Q. Travel Services. Establishments providing travel information and reservations to individuals and businesses. This classification excludes car rental agencies.

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